

The Port of Dubuque Master Plan



America's River
AT THE PORT OF DUBUQUE



THE CITY OF
DUBUQUE
Showing the Spirit.

Port of Dubuque Master Plan

Dubuque, Iowa

Prepared for
The City of Dubuque, Iowa

Prepared by
URS Corporation
Leland Consulting Group
EDG, Ltd.

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I. Introduction

The Master Plan for the Port of Dubuque represents the culmination of a ten-month community planning and design effort to craft a comprehensive, long term vision and redevelopment concept for the 4th Street Peninsula and the South Ice Harbor. The planning process has been directed by the 4th Street Peninsula Work Group, a committee comprised of project area stakeholders and City staff. A variety of committee and public participation opportunities were employed in the planning process including monthly stakeholder luncheons, design workshops, site tours, regular committee meetings, City Council work sessions and a public open house. The plans and development standards have been continuously refined based on the input received during the planning process.

A three part process has been utilized in the development of the master plan and design standards:

Part One: Physical and Market Analysis

Part Two: Conceptual Design

Part Three: Final Design

Parts One and Two were described in two previous documents. This document focuses on the final design efforts, including the master site plan and the final design standards.



Aerial View of Master Plan

II. Final Design Summary

The final master plan is based on a refinement of the previous “Central Green” concept plan. Of the three concept plan alternatives, this plan was found to be the most expedient and least costly to implement because it closely followed the alignment of the existing major streets as well as many of the current ownership patterns. The plan also provides the most downtown-like pattern of interconnected streets and blocks. This proposed arrangement provides a high degree of flexibility and a wide variety of build-out options. Consequently, the plan meets the six primary redevelopment objectives developed by the work group:

- Capitalize on previous and upcoming investment
- Provide a complementary mixture of uses
- Build on activity of existing and previously planned uses
- Create a pedestrian and transit oriented environment
- Use downtown as a model for street and block patterning
- Develop the area as a new neighborhood of downtown
- Balance creative vision and market demand

The overall master plan is organized in a series of interrelated and supportive plan elements:

- Regulating Plan
- Thoroughfare Plan
- Pedestrian Circulation Plan
- Built Form Plan
- Illustrative Plan
- Phasing Plan
- Phase I Plan

The Regulating Plan designates the types and distribution of new uses. In general, the plan promotes a wide mixture of uses while allowing for market flexibility. Three categories of mixed-use are proposed for the project.

The Mixed Use - 1 designation applies to areas of the project that are best suited for business and employment oriented uses such as office/showroom, institutional, office and commercial. These use areas are most closely situated near the existing highway and railway systems and the development blocks are upwards of 3 acres in size.

Mixed Use - 2 allows the widest range of uses from entertainment, office and retail to multifamily residential. The designated uses are supportive of the other use designations as well as the existing and new attractions. The wide variety of use also provides a high degree of market flexibility. Development parcels are in the 2-3 acre range.

The Mixed Use - 3 designation is applied to areas along 5th and Bell Streets and is intended to promote a more commercial, main-street environment. The design standards support this concept by requiring that a minimum of 60% of the ground floor area of buildings facing 5th and Bell Streets be dedicated to business service, retail and restaurant/lounge or cafe uses. Development parcel size is typically two to three acres with the exception of the property owned by the Diamond Jo Casino which is a little over five acres.

The Thoroughfare Plan designates three street types: parkways with 73 foot rights-of-way and parallel parking, local streets with 60 foot right-of-way and parallel parking and local streets with 50 foot rights-of-way without parking. These streets are depicted in the illustrative cross sections.

The Pedestrian Circulation Plan illustrates two levels of circulation: public sidewalks and trails and semi-public sidewalks. The plan connects the two previously isolated subdistricts, South Port and North Port by a new river and harborwalk open space system. Additional pedestrian open space features include a new Ice Harbor marina, a central green and a linear art park. The plan also provides for pedestrian circulation improvements to the existing street connections between the Port of Dubuque and the adjacent downtown. Most notably is the addition of a new sidewalk to be constructed along the north side of the 3rd Street overpass and an extension of the riverwalk across the top of the Ice Harbor floodgates.

The Built Form Plan illustrates the desired pattern of building forms. The primary goal is to bring buildings up to the sidewalk line or near the waterfront and locate new parking areas to the rear and sides.

The complete project build-out for both the south and north port areas is depicted in the Illustrative Plan. On the North Port, the commercial and office uses are organized along both sides of 5th Street. Mixed office, commercial and residential uses are organized along the new secondary, local streets. Perspective views also illustrate the proposed character of new development shown in the plan. Several areas are designated for sidewalk vendors to operate throughout the year.

The plan illustrates conversion of some existing surface parking to new development; however, current land values and a high water table make it impractical to suggest placing the parking underground or in large, multilevel ramps. Recognizing that surface parking will remain a significant use in the project area, design standards were developed to require that extensive landscape and streetscape treatments be incorporated into both existing and new parking areas to make them more park-like and visitor-friendly.

The South Port area suggests an expansion of the Ice Harbor to accommodate a new marina-oriented development. Access between the North and South Ports areas is improved via a new local street connection along the west side of the Ice Harbor.

The suggested sequence of implementation is illustrated on the Phasing Plan. The North Port neighborhood is planned to develop over the next seven to ten years followed by the South Port neighborhood. The total project build out for both the North and South Port areas is anticipated to take approximately 18 to 20 years.

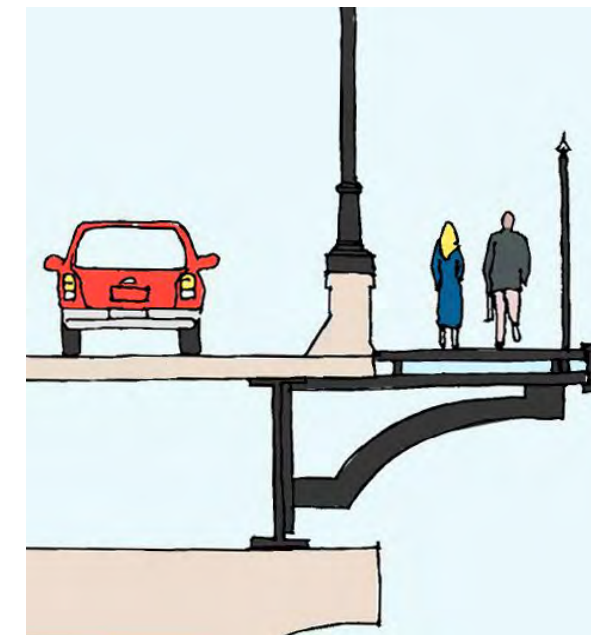
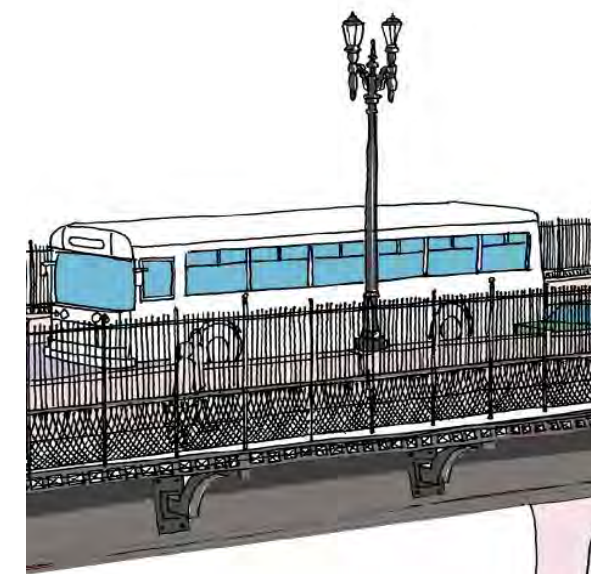
The primary focus of the first phase of redevelopment is illustrated on the Phase I Plan. The plan shows several new mixed-use buildings and an all-season transit station surrounding a new central green space. The green space is strategically located across from the new Education and Conference Center entry plaza allowing the City to use both these open spaces for special events.

Conceptual designs were developed for a new 3rd Street overpass entry feature, project identity signs, reusable predevelopment screening, a public art park or sculpture garden and an extension of the Mississippi Riverwalk along the South Port area.

The designs for the 3rd Street overpass entry feature build on several of the entry monument and signage concepts as well as the industrial and maritime history of the 4th Street peninsula and Ice Harbor area. They also provide vertical circulation for pedestrians visiting the National Mississippi River Museum and Aquarium.

The plan also calls for new decorative treatments to be added to the 3rd Street overpass structure including a 6-foot wide walkway along the northerly side. Other aesthetic improvements include replacing the chain link protective barrier with ornamental guardrail and repainting the steel under-structure and existing decorative lights in dark gray and black to match the other proposed streetscape elements.

Several conceptual design alternatives have been developed for monumentation and project signage. These are intended to illustrate a range of vertically oriented ideas to capture attention from a range of distances. The designs utilize local limestone, painted metal and a variety of lighting effects.



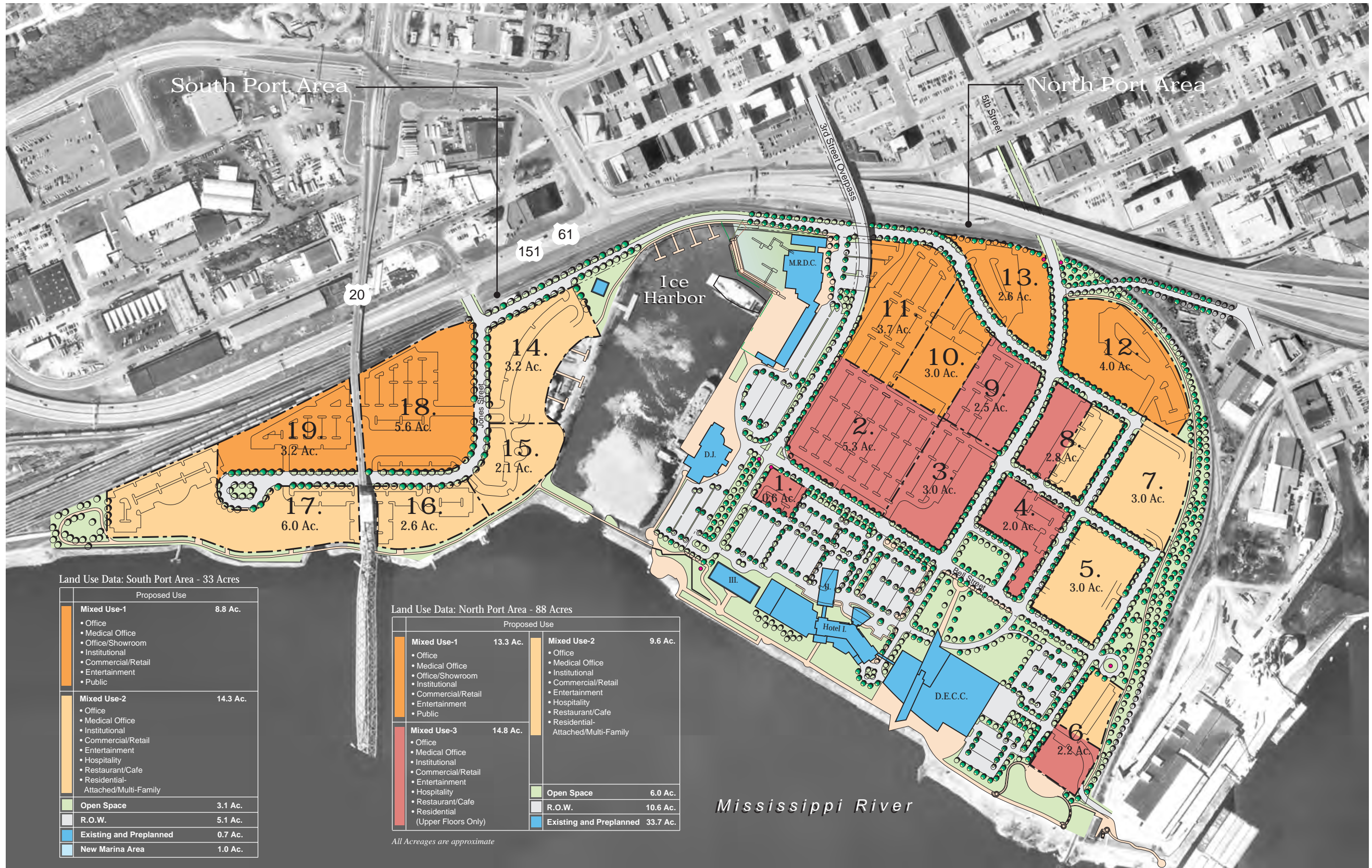
Partial View of Phase I Plan

3rd St. Overpass Northside Walkway Concept



Project Area

4th Street Peninsula and South Ice Harbor Master Plan



Regulating Plan

The Port of Dubuque Master Plan



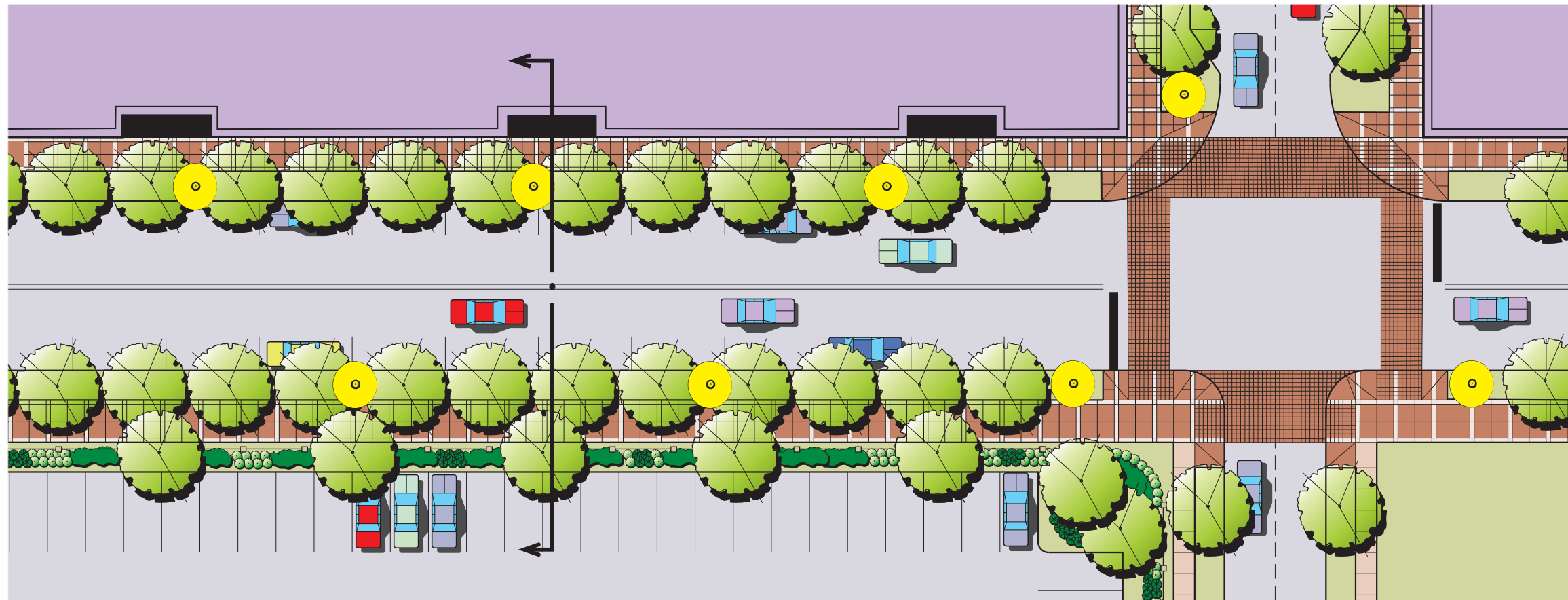
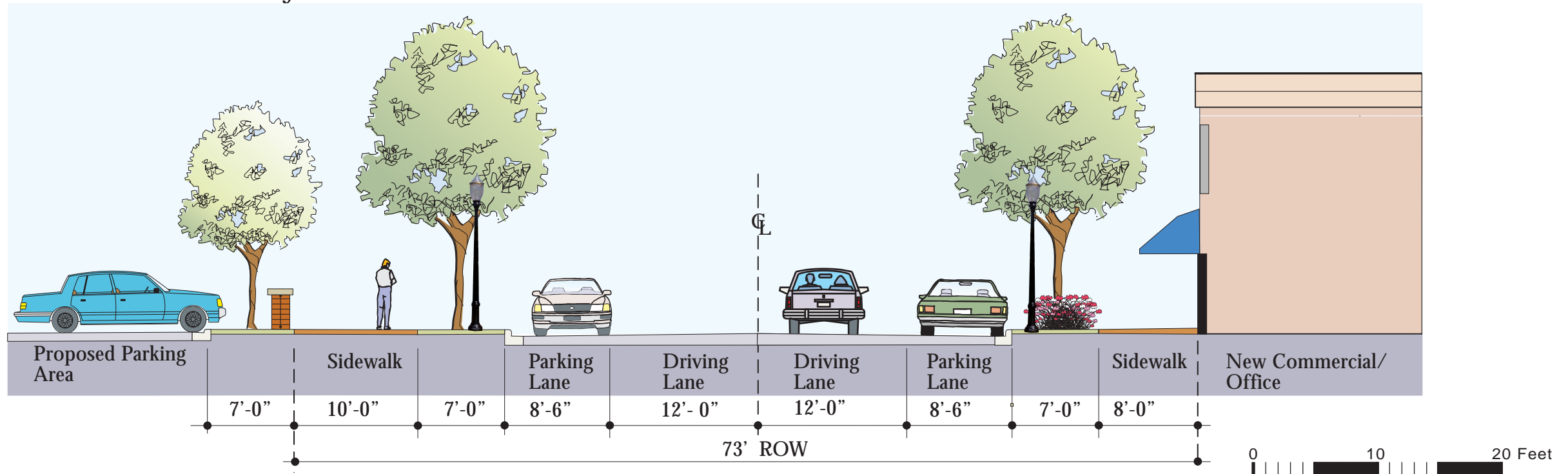


- 50 Foot R.O.W.
- 60 Foot R.O.W.
- 73 Foot R.O.W.

Thoroughfare Plan

The Port of Dubuque Master Plan

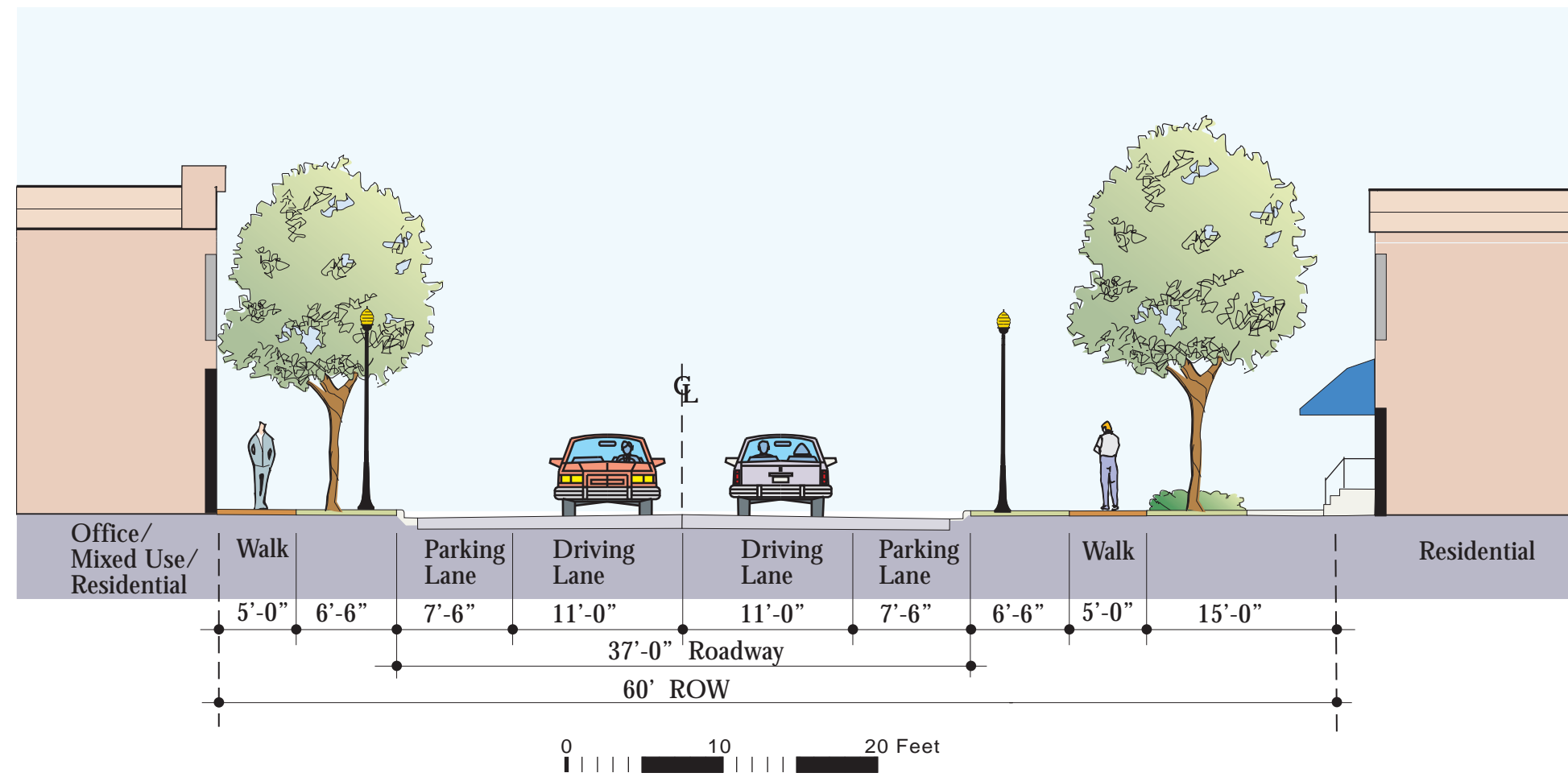
Bell Street "Parkway"



Parkway Section & Plan

The Port of Dubuque Master Plan

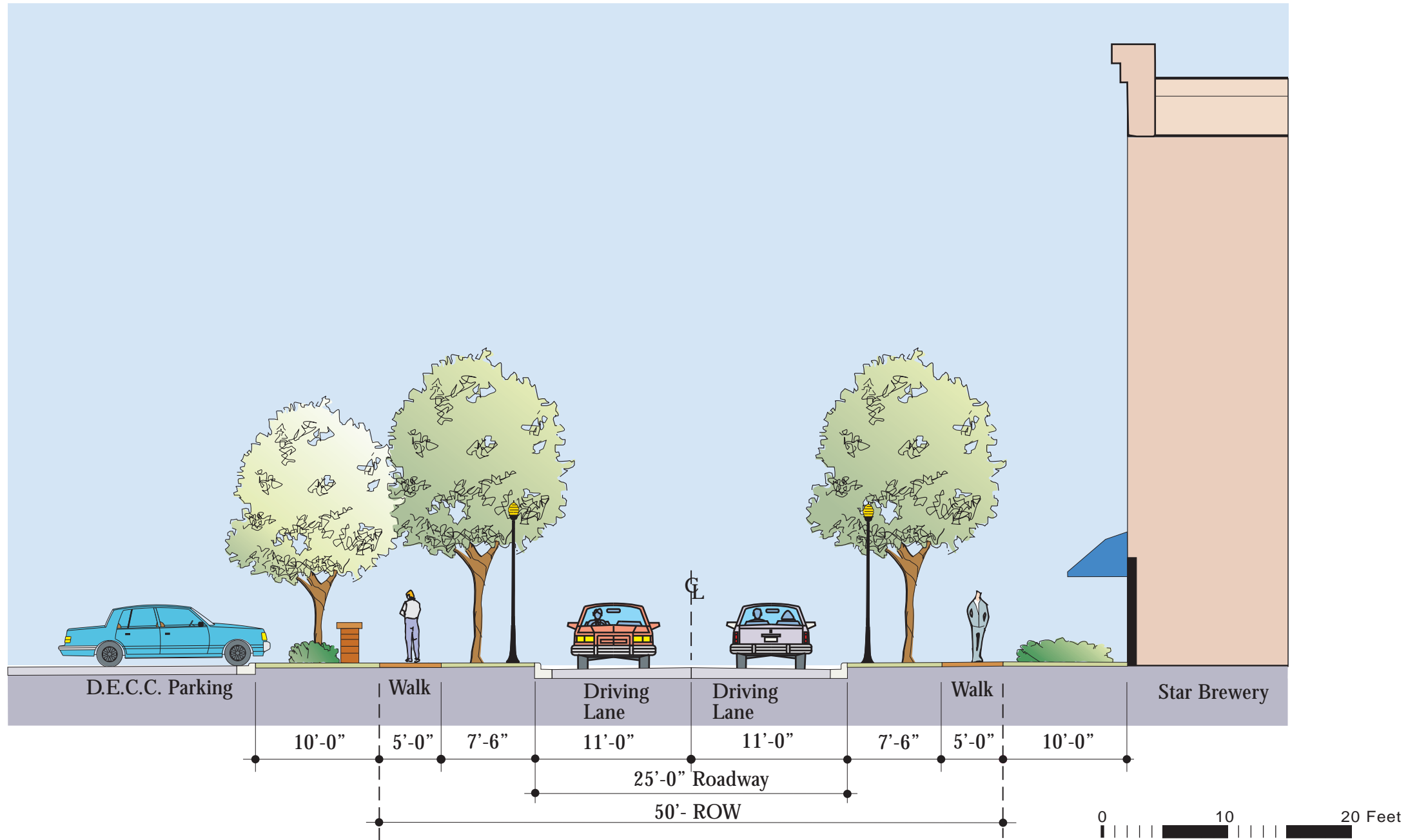
Local Street



Street Section

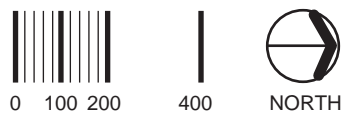
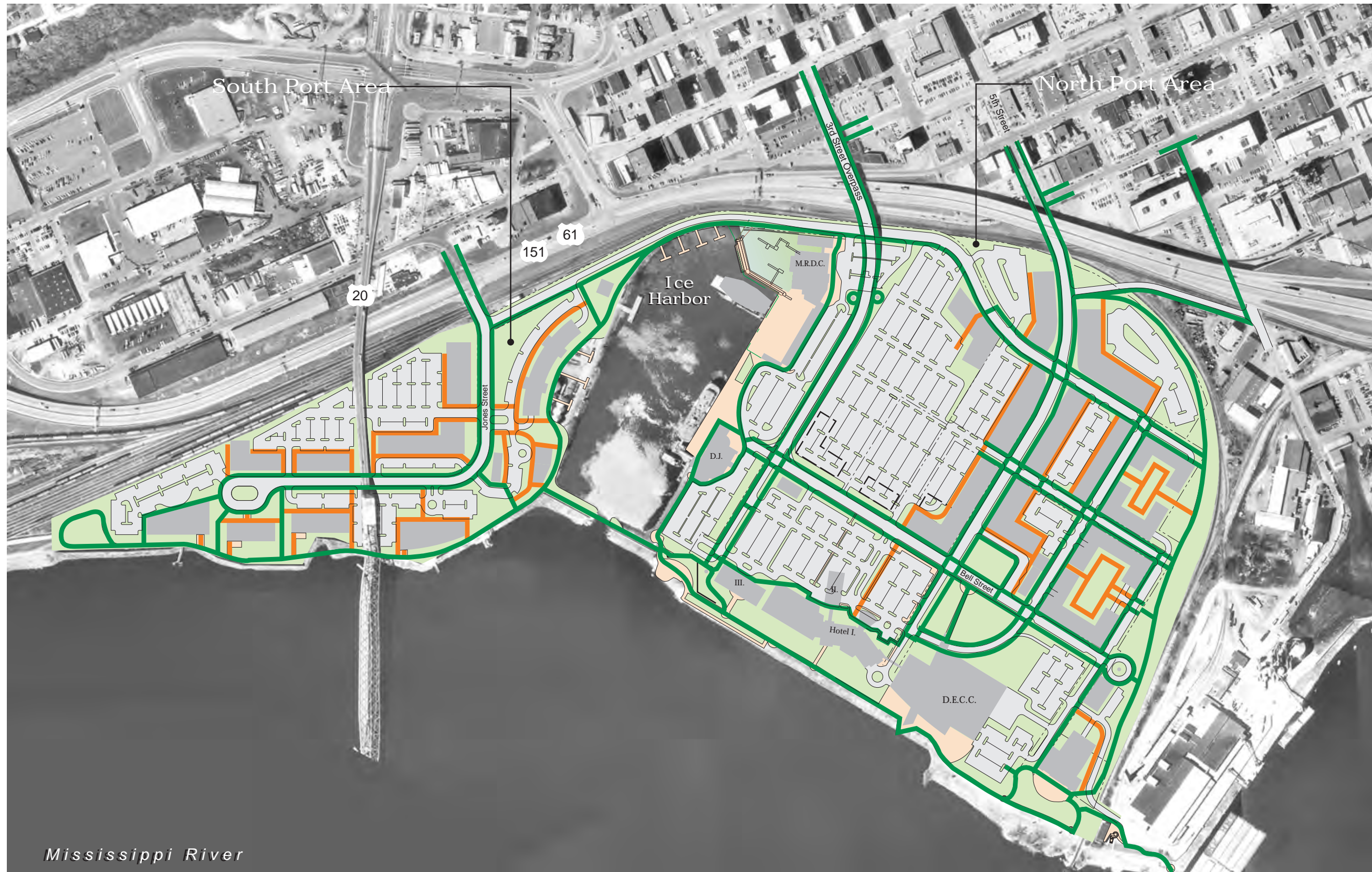
The Port of Dubuque Master Plan

Local Street with No Parking



Street Section

The Port of Dubuque Master Plan



- Public Sidewalks and Trails
- Semi-Public Sidewalks

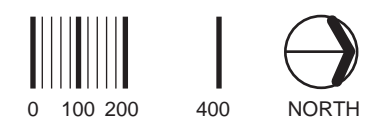
Pedestrian Circulation Plan

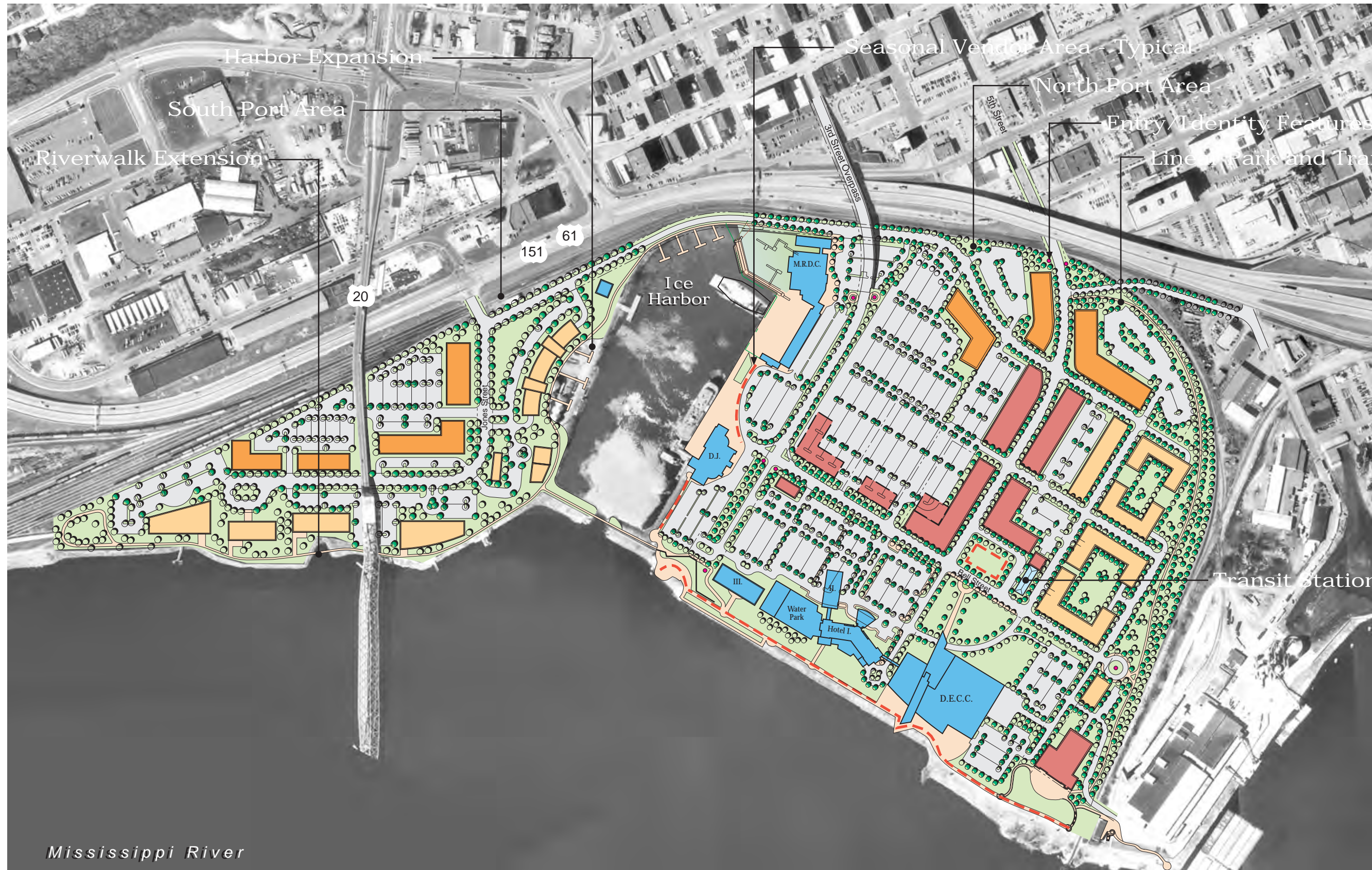
The Port of Dubuque Master Plan



Built Form Plan

The Port of Dubuque Master Plan





Mississippi River



Illustrative Plan

The Port of Dubuque Master Plan



Aerial View Looking Southeast

The Port of Dubuque Master Plan



View Looking East on 5th Street
The Port of Dubuque Master Plan



View Looking North at Bell and 5th Streets
The Port of Dubuque Master Plan



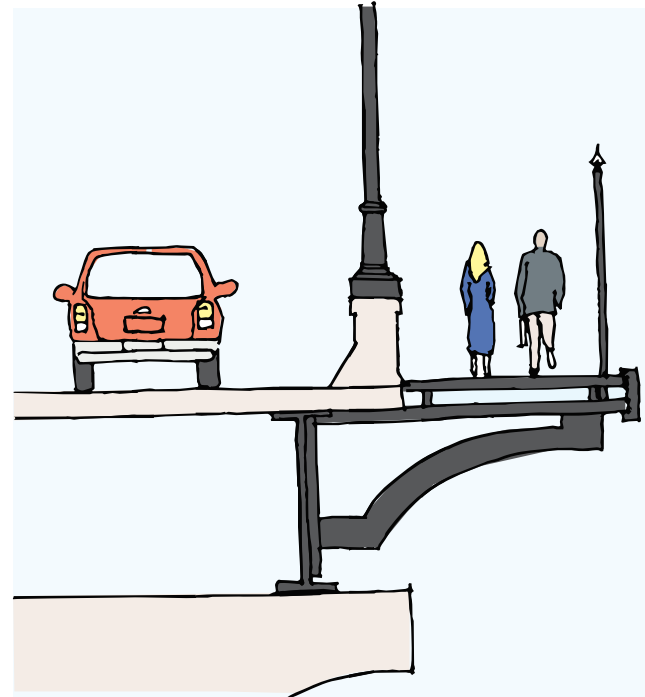
Phase I Plan

The Port of Dubuque Master Plan

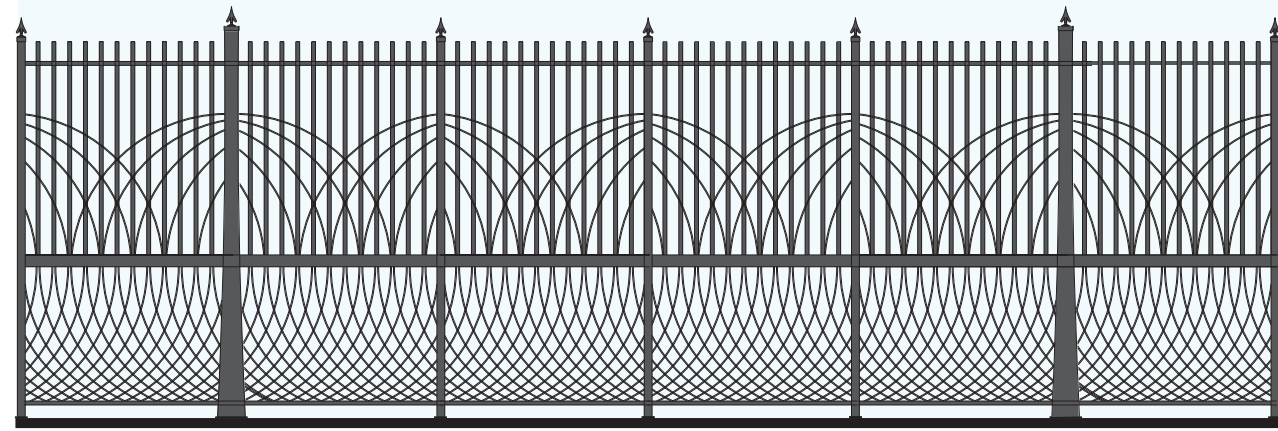


Phasing Plan

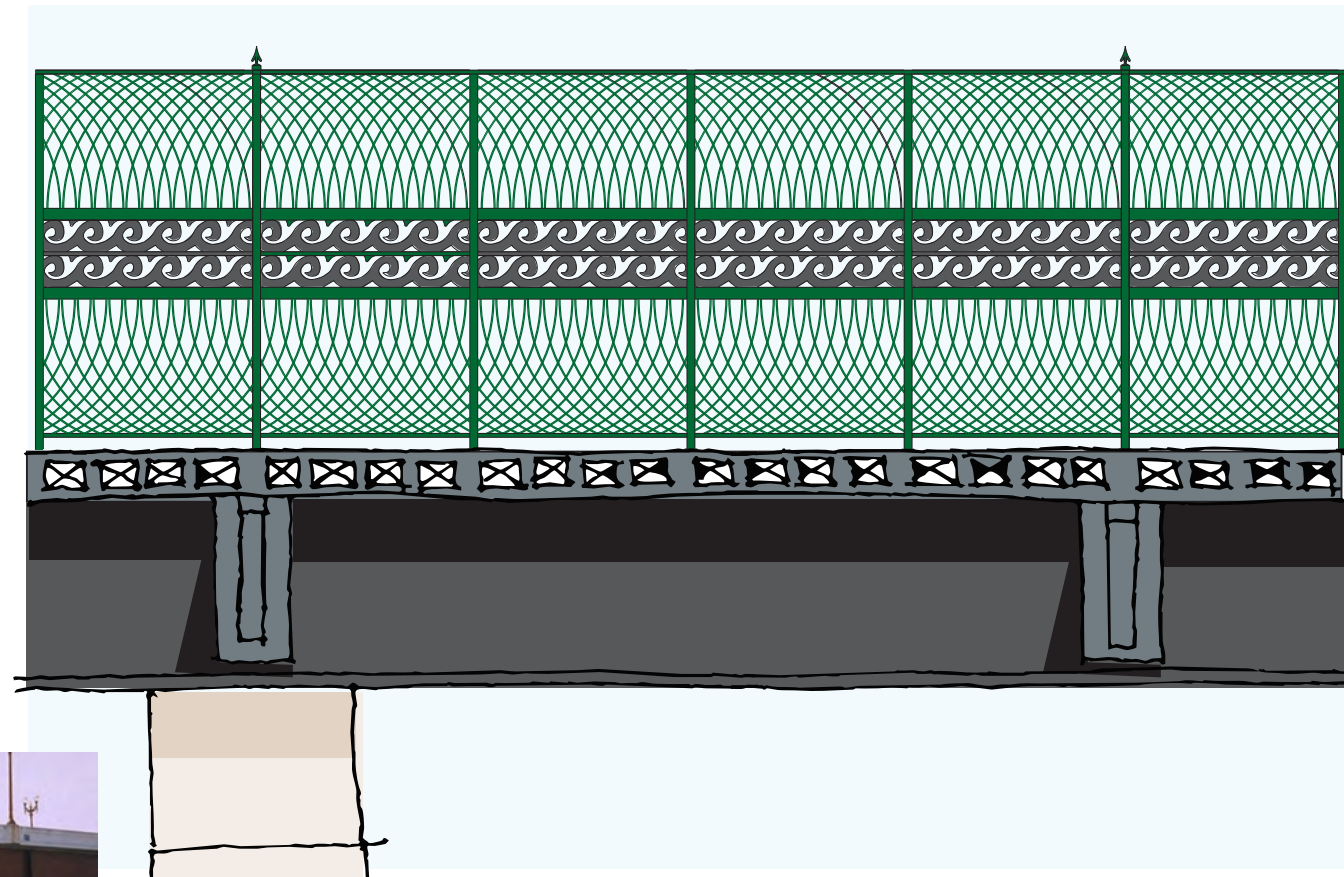
The Port of Dubuque Master Plan



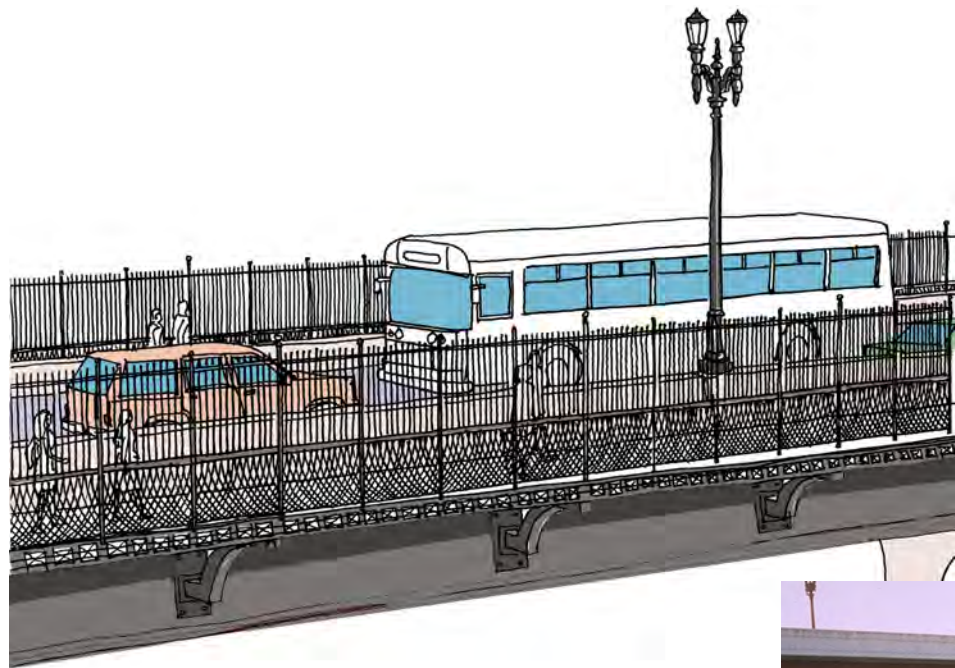
Overpass Fence/Pedestrian Walkway Section



Overpass Ornamental Fence Elevation Option A



Overpass Ornamental Fence Elevation Option B



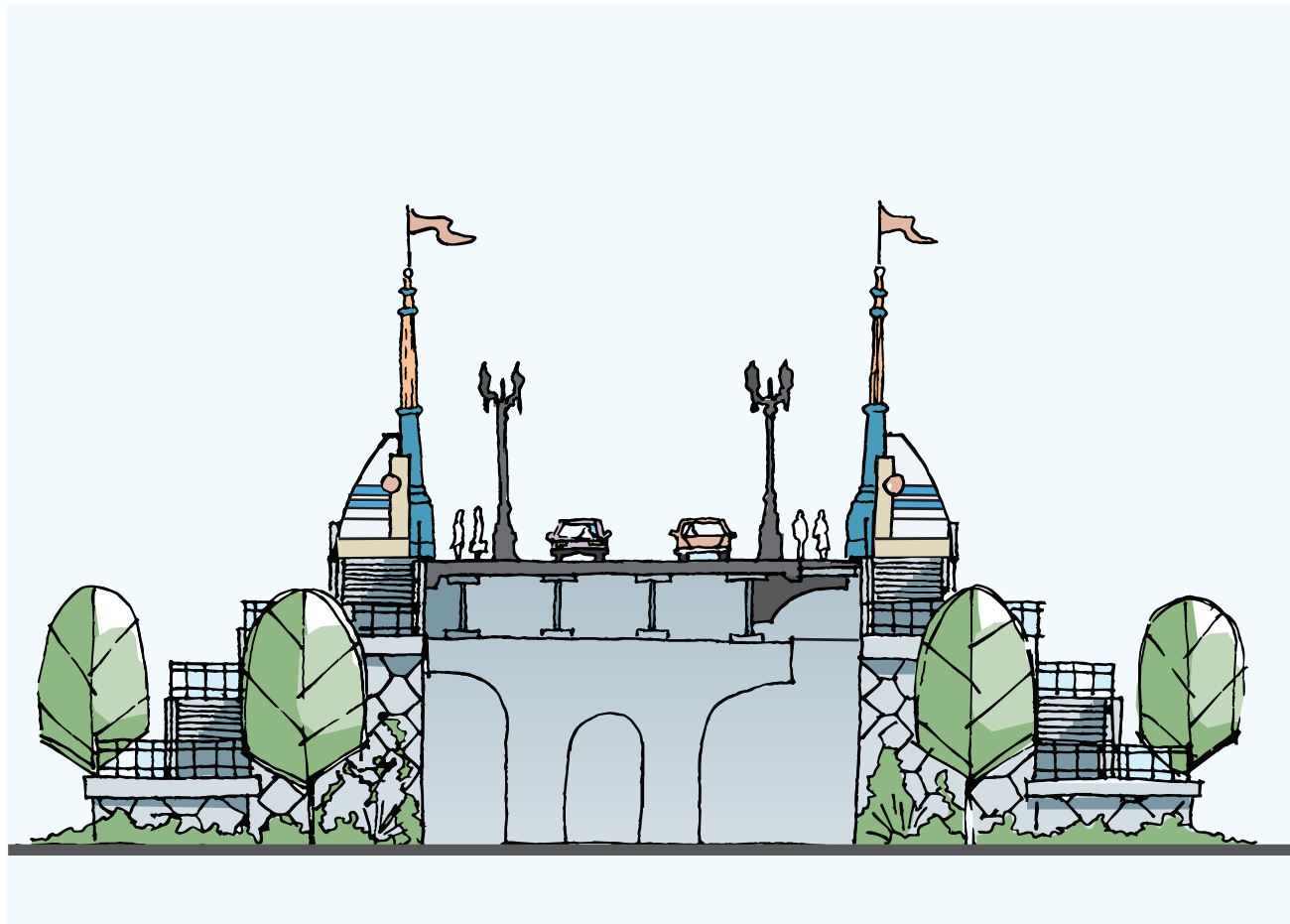
Fence / Pedestrian Walkway



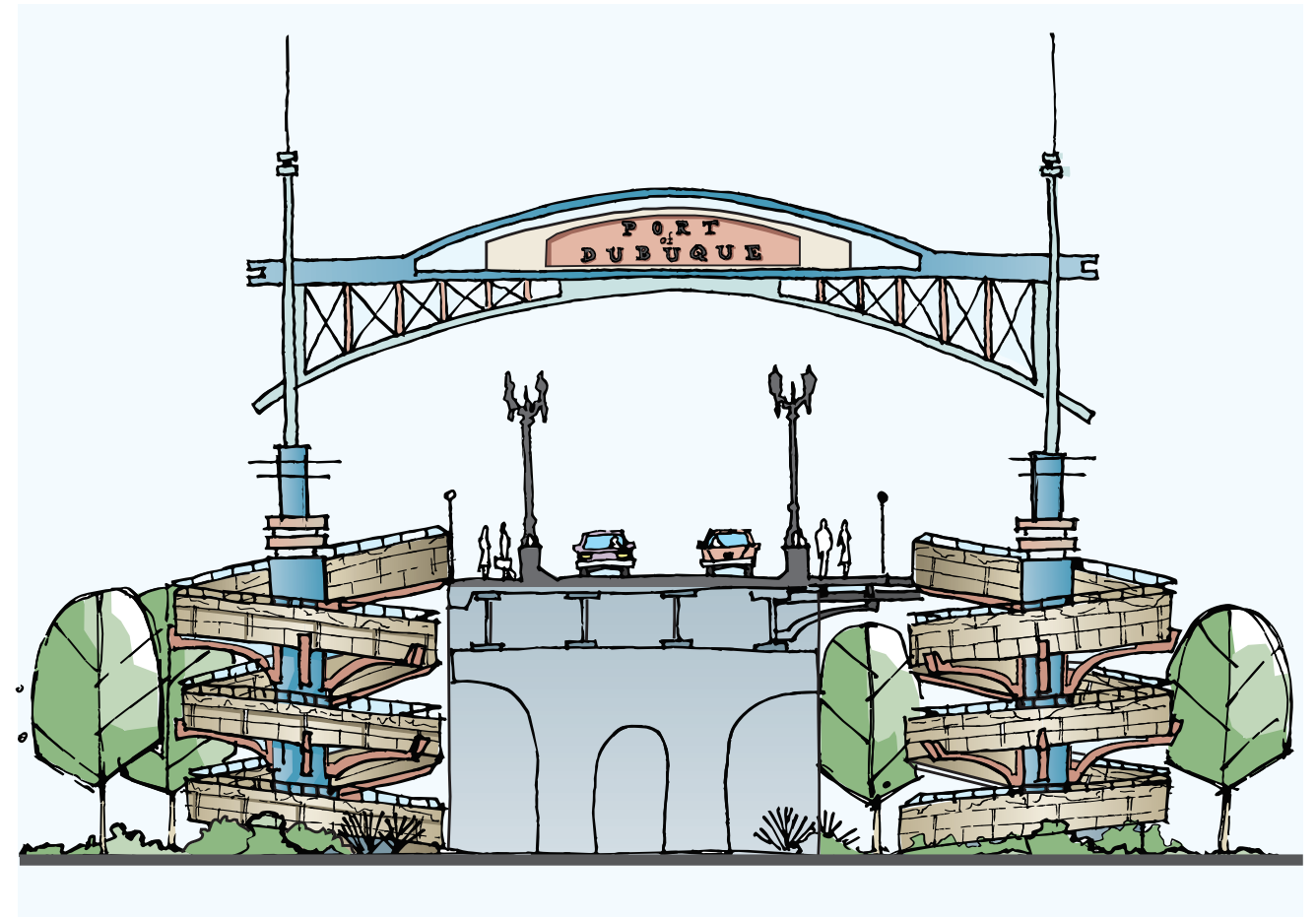
Photo of Existing Overpass

3rd Street Overpass Pedestrian Enhancements

The Port of Dubuque Master Plan



Gateway Concept Using Stairway Connections



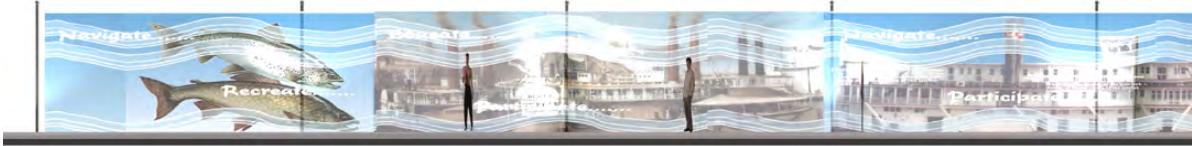
Gateway Concept Using Ramp Spirals

Gateway Concepts

The Port of Dubuque Master Plan



Existing Conditions

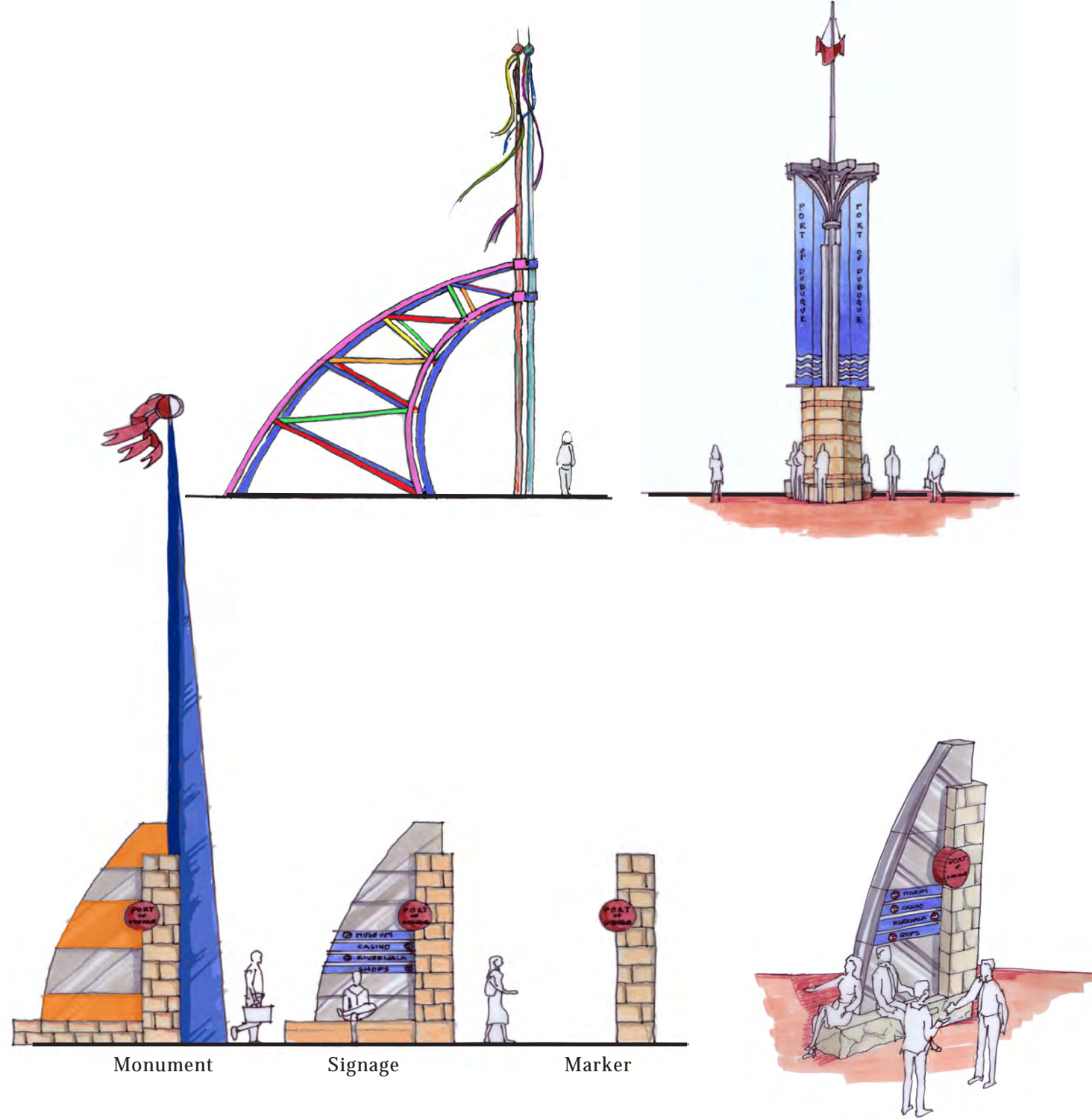


Proposed screen concept



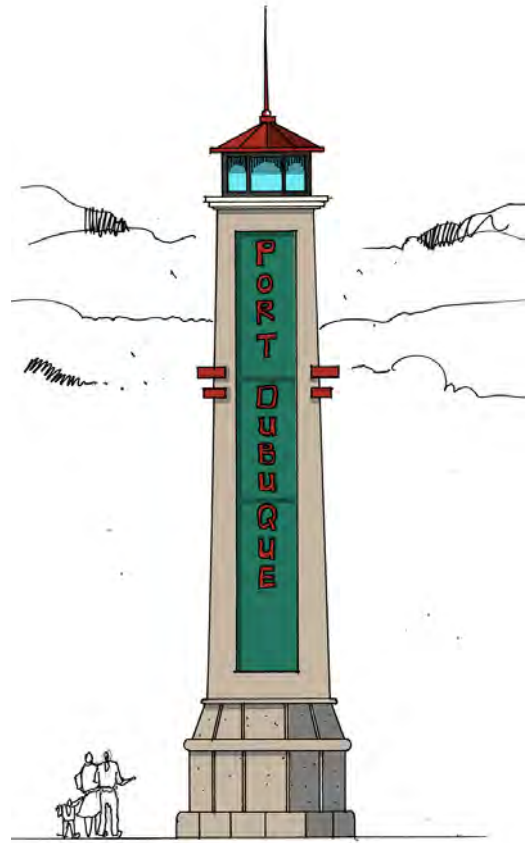
Proposed translucent "scrim" with Port theme graphics, 10 ft. high +

Pre-development Site Screening Concepts

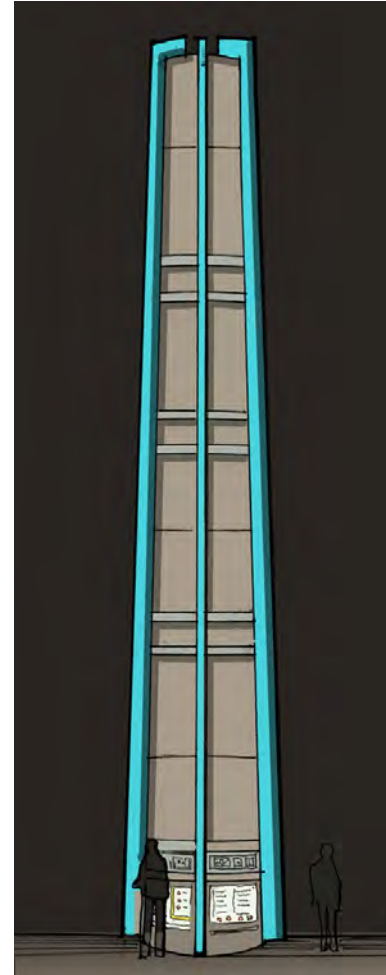


Entry Monument Concepts

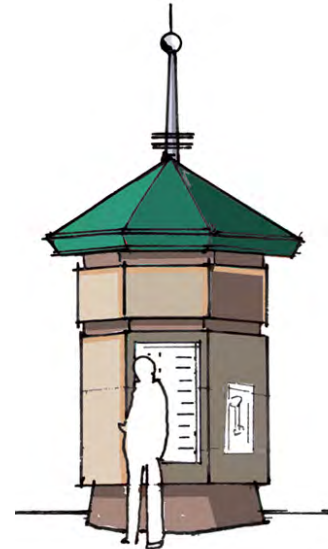
Screening and Monumentation
The Port of Dubuque Master Plan



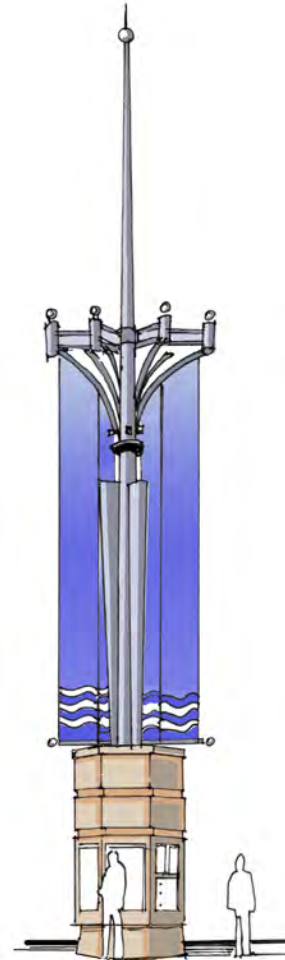
Concept 1.



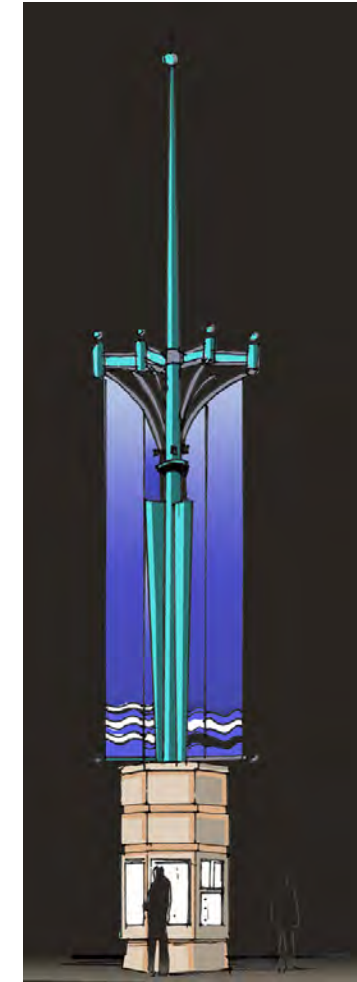
Concept 2. Night View



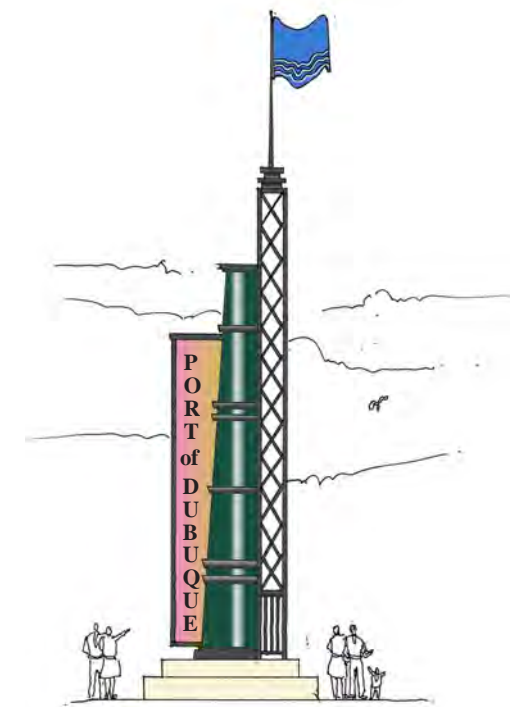
Concept 3. Kiosk



Concept 3.



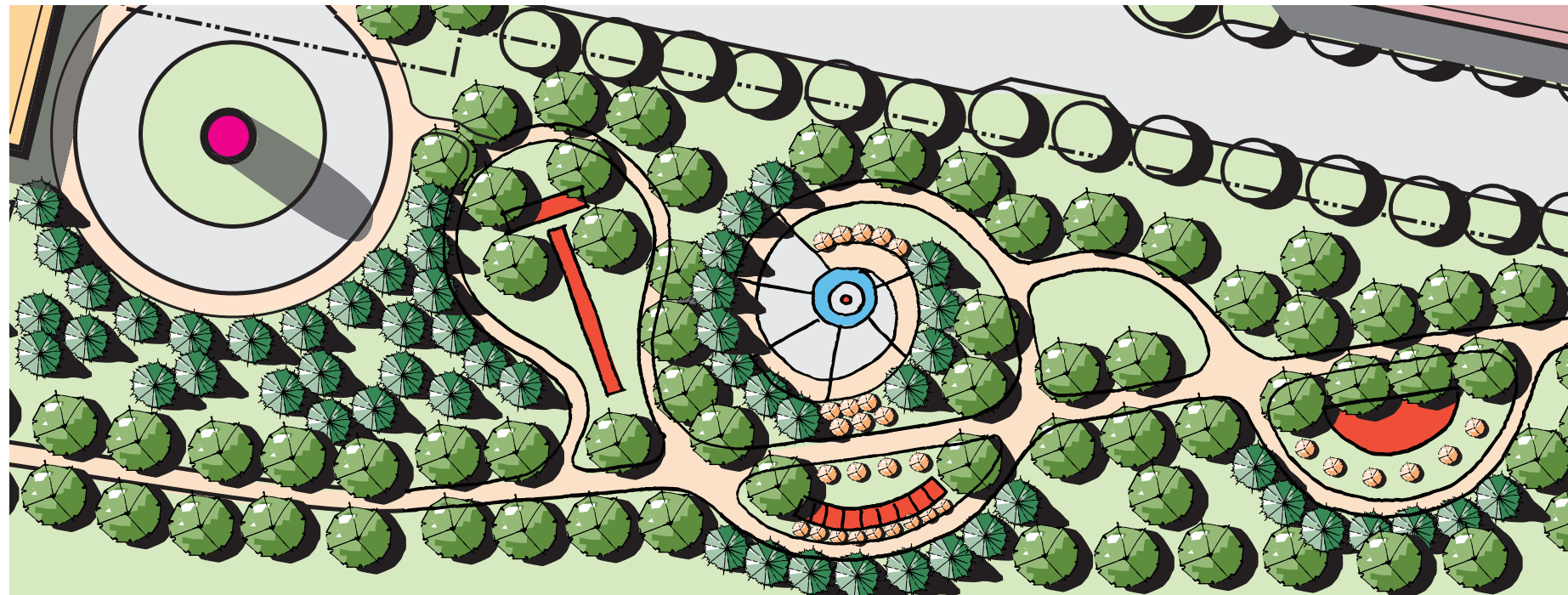
Concept 3. Night View



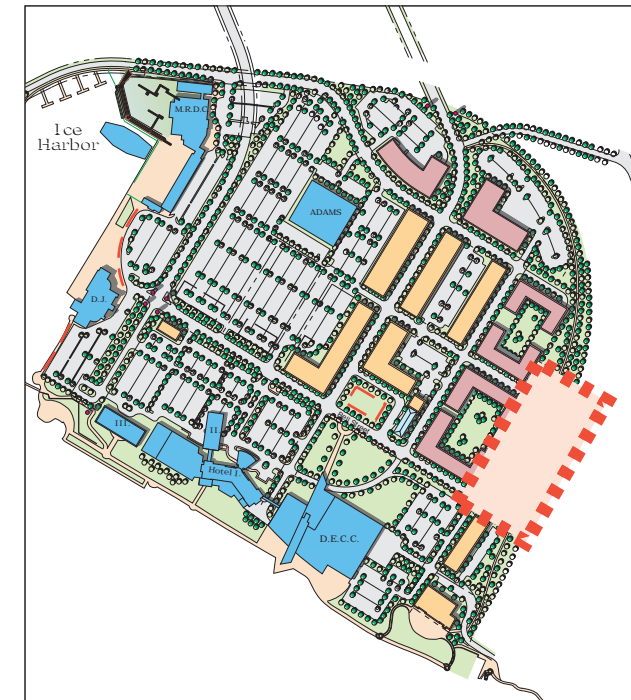
Concept 4.

Monumentation Concepts

The Port of Dubuque Master Plan



Illustrative Sculpture Garden Plan



Sculpture Garden Location Map



Illustrative Sculpture Garden Section/Elevation



Art Examples in Park Settings

'Art Park' Open Space Concepts

The Port of Dubuque Master Plan

DESIGN STANDARDS



III. DESIGN STANDARDS

The design standards for the Port of Dubuque have been developed to ensure that new and existing facilities work together to create an attractive, high quality, pedestrian-oriented urban neighborhood for visitors, residents and workers. These standards are both prescriptive and descriptive in nature. They prescribe specific minimum requirements for elements such as parking lot landscaping and signage as well as describing parameters for the design of new buildings and the remodeling of existing buildings. A pictorial “design vocabulary” is included to provide a set of “good” examples. Many of these images are of facilities from the Dubuque metropolitan area.

Failure to comply with these design standards and procedures constitutes a violation of the Ice Harbor Urban Renewal District Plan, adopted March 4, 2002.

Applicability

These design standards apply to the following:

- New and existing parking facilities
- New buildings and vessels
- New site development
- New public and private open space
- All sites, exclusive of their existing buildings or vessels, and including parking, outdoor storage and perimeters
- Painting of 25% or more of existing building exterior
- Remodeling/renovation of existing building or vessel exterior which requires a building permit (exclusive of roof repair)
- Interior remodeling/renovation of 50% or more of existing building floor area
- Renovations of existing sites
- Expansions of existing facilities, buildings or vessels which require a building permit

With respect to existing facilities, the intent is to tailor the application of the standards proportionately to the degree of the change proposed. The greater the degree of change, the greater the degree of compliance that will be expected. Minor improvements may be made to existing uses without costly upgrades or a complete makeover of the site.

Design Review

All property owners and/or developers shall meet with a representative from the City Planning Services Department to discuss the interpretation and application of these design standards to existing sites and any prospective projects to which these design standards apply.

Following this meeting, all compliance projects, improvements, additions and new facilities including proposed parking strategy shall be described in narrative text and illustrative engineering and architectural drawings, including the following:

- Colored front and side building elevations (1/4” scale min.)
- Colored illustrative site landscape plan (1:30 scale min.)
- Dimensioned site plan (1:30 scale min.)
- Dimensioned site lighting plan (1:30 scale min.) indicating proposed illumination patterns and light levels
- Dimensioned architectural plans, including building elevations, cross sections, floor plans and details
- Exterior construction materials samples, including brick, stone, glazing, windows and doors, signage materials, fencing, etc.

These application materials shall be submitted as a complete package to the Planning Services Department. Incomplete applications will not be processed. Applications will be reviewed by the 4th Street Peninsula Work Group, who will provide a recommendation to the City Manager.

Design Approval

Design approval, based on a review of the application materials identified above, will be issued by the City Manager. A building permit will not be issued by the Building Services Department until such approval is given in writing.

The following new facilities have been subject to comprehensive design review by the City and members of the 4th Street Peninsula Work Group:

- National Mississippi River Museum and Aquarium
- Grand Harbor Resort and Waterpark
- Mississippi River National Education and Conference Center

These facilities form the core of the *America’s River* project and are considered “icon projects.” In many respects, they have raised the bar and set a level of design integrity and construction quality that are the foundation of these design standards. As such, these facilities are exempt from the Built Form section of these standards.

Implementation

Application of these design standards to new facilities shall be effective as of March 4, 2002.

Application of these design standards to existing facilities shall be effective as of March 4, 2002 and shall be implemented according to the following 18 month schedule:

0-6 months Conduct property review meeting with the Planning Services Department.

6-12 months Submit necessary plans and information describing compliance plans and improvements to the Planning Services Department.

12-18 months Complete necessary modifications and improvements.

The City Manager may modify the schedule or application of these standards. A property owner who is aggrieved by the City Manager’s decision may apply to the City Council for review thereof. The City Council may then modify the schedule or the application of these design standards.

IV. BUILT FORM

Minimum First Floor Elevation

To ensure that development is built consistent with district-wide storm water detention parameters, the minimum first floor elevation of all new buildings and structures shall be set at an elevation of at least 605.5 feet.



Building Context and Style

New buildings should relate to the traditional buildings common in the downtown. This can be achieved by maintaining similar setbacks, building scale and height, cornice lines, fenestration patterns, architectural styles and details, building materials and colors. The prevailing building vocabularies in downtown Dubuque are traditional "main street" style commercial structures up to four stories; five to ten-story office buildings with ground floor commercial uses designed in a variety of classical revival styles; mid-rise, warehouse style manufacturing facilities; and residential row houses up to four stories, predominantly in French and Italian revival styles. Although new buildings may be designed in a variety of styles, they should draw upon the design features found in existing, exemplary downtown structures. These features are illustrated in the Design Vocabulary in the Appendix.

Ground Floor Uses

In order to promote street life and pedestrian activity, at least 60% of the ground floor frontage facing Bell Street and 5th Street shall be comprised of commercial, retail, entertainment, restaurant or business service uses.

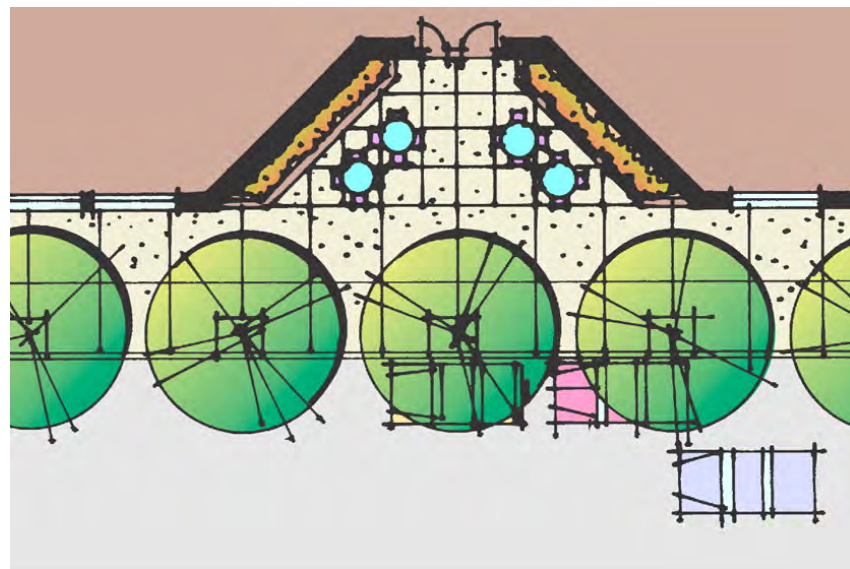


Building Setback/ Build-to Line

New buildings shall meet the defined public sidewalk line except for small setback areas (10-15 feet in depth) to create entry courtyards, patios, or outdoor seating, dining and gathering areas.

New residential buildings shall be set back from the public sidewalk line or right-of-way a minimum of 10 feet and a maximum of 20 feet to provide semi-private transition space between the public street and the front entry. This transition space shall be landscaped.

See Sidewalk Landscaping.



Setback along sidewalk line to provide entry court and outdoor seating area.

Building Height

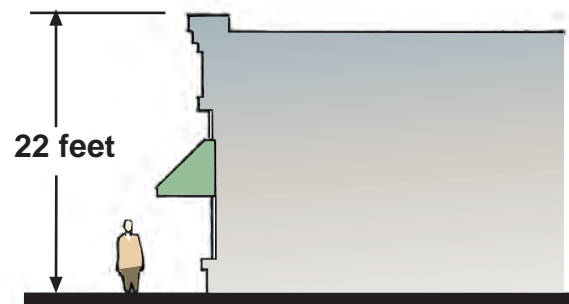
Building heights shall vary based on their proximity to the water front, with taller buildings located adjacent to the river and the harbor to capitalize on views and maximize land values.

Waterfront

- New buildings adjacent to the Ice Harbor or Mississippi River shall be a minimum of 3 stories (36 feet) and a maximum of 10 stories (112 feet) in height.

Non-waterfront

- In general, non-waterfront buildings shall be a minimum of 2 stories (22 feet) up to a maximum of 10 stories (112 feet) in height.
- New freestanding restaurants and office-showroom buildings may be of one-story construction and shall be no less than 22 feet in height to the top of the front and side cornice lines.
- Any new building located across Bell Street from the Education and Conference Center shall not obscure the view of the dome of the historic County Courthouse as viewed from inside the central corridor of the Education and Conference Center.



Single story building with 2 story cornice height.

Ground Level Expression

In commercial, office and mixed use buildings, the ground floor shall be distinguished from the floors above by the use of one or more of the following elements: horizontal banding, an intermediate cornice line, a change in building materials, an awning or an arcade.



Examples of effective differentiation between ground floor and upper levels.

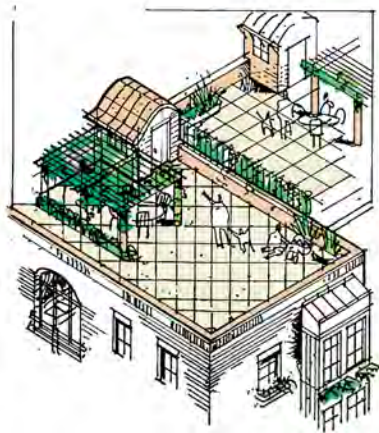
Roof Lines

Flat roofs are the most common and traditional roof form. New buildings should use flat roofs, although gable, hip or traditional mansard roofs are permitted.

On flat roofs, rooftop terraces, decks and gardens are encouraged, to provide views to the river and cityscape and improve the view from taller, adjacent structures. Arbors or other roof top structures may be exempt from height restrictions if they meet the following conditions:



- A maximum height of 12 feet above the roof deck
- A setback from all building facades fronting a public street of at least 10 feet or equal to the structure's height, if greater
- Enclosed structures shall cover no more than 20 percent of the roof area
- Arbors or other vertical "open" structures shall cover a maximum of 80 percent of the roof area

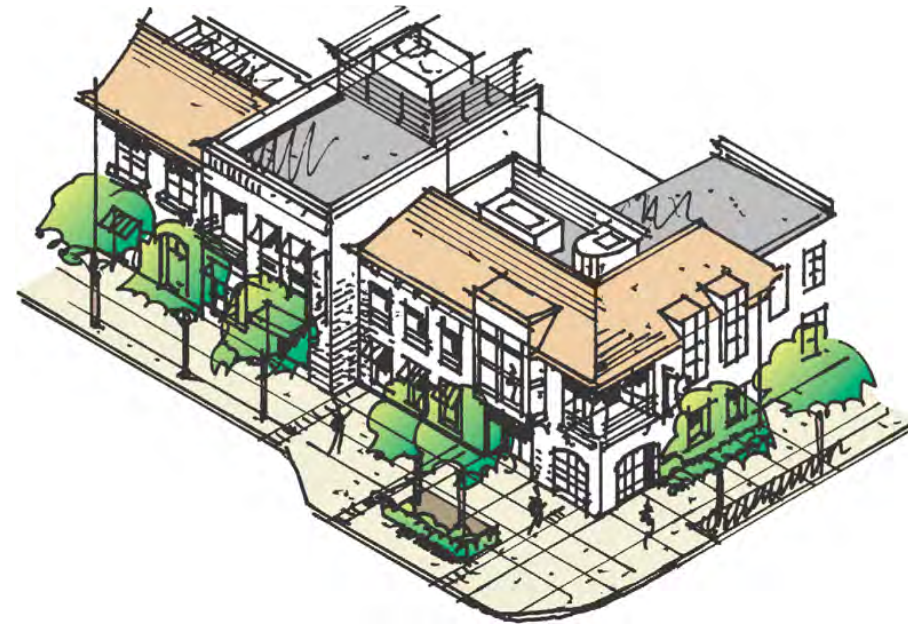


Rooftops developed as usable outdoor space.

Screening of Rooftop and Mechanical Equipment

All mechanical equipment shall be screened from street frontage view with materials that are architecturally compatible with the primary building facades. Screening provided by the roof structure or by other architectural elements is preferred.

Rooftop equipment may also be screened by a smaller accent roof or enclosure. If this structure is set back from the primary facade a distance equal to its height, this structure may exceed the building height limit.



Examples of rooftop equipment screening.

Building Width

Nonresidential buildings more than 30 feet in width shall be divided into smaller increments through articulation of the facade. This can be achieved through a variety of techniques such as the following:

- Divisions or variations in materials (although materials should be drawn from a common palette)
- Window bays
- Separate entrances and entry treatments or porticos
- Variation in roof lines
- Awnings
- Variations in brick or stone work
- Colonnade or arcade



Large buildings effectively broken down in smaller pedestrian scale increments.

Facade Transparency

Where commercial or office uses are located on the ground floor, a minimum of 25 percent of the ground level facade fronting a public street and a minimum of 20 percent of the ground level side and rear facades shall consist of either windows or doors to allow views into and out of the building's interior.

A minimum of 15 percent of the front, side and rear facades of all upper floors of office uses shall be transparent via windows and/or balcony doorways.

A minimum of 10 percent of the front facade of all upper floors of commercial, retail, restaurant and entertainment uses shall be transparent via windows and/or balcony doorways.

Entries

The main entrance shall always face the primary street or street corner with secondary entrances to the side or rear.

The main entrance of commercial, office, and mixed-use facilities shall be placed at sidewalk grade.

Balconies and Terraces

Upper floor balconies and terraces are encouraged to provide opportunities for outdoor activities, providing views and enlivening the port area.

Balconies fronting public streets may project into the public right-of-way/sidewalk zone a maximum of 5 feet. Balcony supports such as columns or posts are not permitted in the public right-of-way. Under no circumstances may balconies project into the travelway.



Effective use of ground level and upper level transparency.



Well defined and accessible entryways benefit visitors and business owners.



Terraces and balconies enhance public safety and enliven the streetscape.

Building Materials

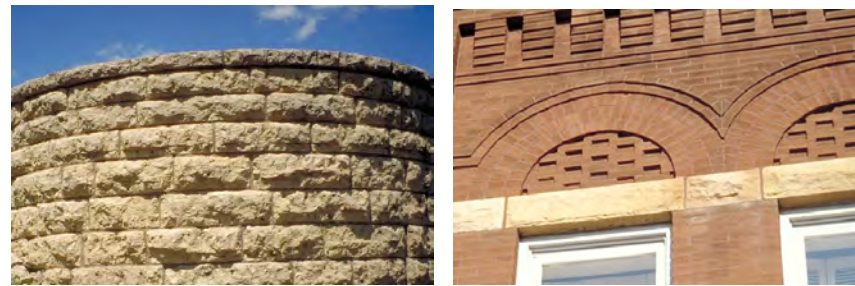
Buildings shall be constructed of authentic, long-lasting materials such as concrete, wood or steel and clad in brick, stone, stucco, and/or architectural metal. Accent materials may include architectural metal trim and panels, glass block, pre-cast concrete, split-face or fractured concrete block, or similar decorative or unique materials.

EFIS or stucco shall comprise no more than 20 percent of any building exterior and shall be located a minimum of 3 feet above grade to prevent discoloration from rain, snow, etc.

Wood or synthetic wood lap siding may be used on residential building types only, up to a maximum of 20 percent of the first story and 50 percent of the upper stories.

The following building materials and building systems are prohibited:

- unadorned or painted standard concrete block
- pre-fab “tilt-up” concrete wall panels
- vinyl, aluminum or fiberglass panel and lap siding
- highly reflective or non-transparent glass
- standard, 3-tab, 270 lb. asphalt shingles
- manufactured metal pole-barn type facilities



Limestone and face brick are commonly used building materials in Dubuque.

Architectural Detailing

Architectural detailing is encouraged to enliven building facades, and to establish a human-scaled, pedestrian supportive environment.

Architectural design shall be sensitive to the massing and proportion of adjacent structures and reflect or compliment the detailing of surrounding buildings, such as lintels, cornice lines, balconies and decorative brick or stone work.



Parking Structures

Parking decks and ramps shall be designed in compliance with these design standards in order to appear compatible with and similar to other nonresidential buildings:

- The ground floor facade abutting any public street or sidewalk shall be designed and architecturally detailed to resemble a commercial/office building.
- Where possible, the ground floor abutting a public street or sidewalk should include commercial or office uses.
- The design of upper floors shall ensure that sloped floors do not dominate the appearance of the facade.
- Windows or openings shall be provided that echo those of nearby buildings and the Design Vocabulary.



Parking structure with commercial storefronts.

Accessory Structures / Buildings

Use of an accessory structure or accessory building for new office, commercial service and residential facilities is prohibited. These facilities shall include a sufficient amount of storage space for the suitable storage of such things as refuse, seasonal maintenance equipment, special event equipment, housekeeping supplies, etc.

New freestanding restaurants, mixed use facilities which include commercial food service or restaurants over 1,500 gross square feet may locate their refuse containers in an accessory structure or accessory building.

The design of the accessory structure shall be in character with the principal building and constructed of the same quality permanent materials as the principal building. The accessory structure shall be located on the same lot and within close proximity to the principal structure. Where such buildings exceed 200 square feet in area, at least 1/3 of the building's exterior perimeter shall be landscaped with ornamental trees and shrubs.

Existing accessory structures which do not meet these standards shall either be removed, replaced with a suitable structure or remodeled using the same quality materials as the principal building. Remodeling may include but not be limited to such things as construction of brick, stone, architectural metal or decorative CMU walls, decorative metal gates and evergreen landscape screening.



Compatible accessory structure.

Franchise Architecture

Franchise architecture (building design that is trademarked or identified with a particular chain or corporation and is generic in nature) is prohibited. Franchises or national chains must follow these guidelines and use the Design Vocabulary to create a unique building that is supportive of the overall urban context being established at the Port of Dubuque.



Typical franchise architecture - unacceptable.



Acceptable franchise design utilizing contextual, community-specific architecture.

Maintenance

All facilities, including vessels, within the Port of Dubuque shall exhibit an exceptional standard of care and quality of appearance. To that end, all properties (buildings, parking lots, landscaping, etc.) shall be maintained in excellent repair at all times including but not limited to:

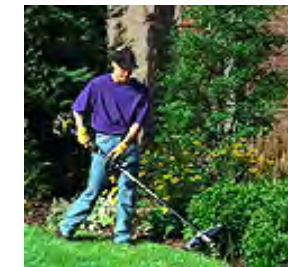
- Exterior materials
- Exterior paint
- Windows and doors
- Awnings and canopies
- Roofing systems
- Eaves and cornices
- Chimneys
- Mechanical and electrical systems
- Exterior walkways
- Surface or structured parking facilities
- Exterior lighting
- Automatic landscape irrigation systems
- Trees, shrubs, flowers, lawns and mulch material
- Building and site signage
- Fences and screening or retaining walls

Facilities shall receive regular maintenance with replacement and repairs budgeted and scheduled over the life of the facility.

Minor building and site repairs shall be made as soon as reasonably possible (within maximum of 30 days) after the discovery of a malfunction, failure or noticeable deterioration.

Major building and site repairs shall be made as soon as reasonably possible after the discovery of a malfunction, failure or noticeable deterioration.

Unhealthy or dead landscape plantings and lawn shall be removed as soon as reasonably possible (within maximum of 7 days) and replaced with suitable new plant materials at the earliest practical time (within maximum of 9 months) based on the appropriate season.



V. PUBLIC REALM

Sidewalks and Walkways

Pedestrian accessibility and continuity shall be provided throughout the area.

Continuous sidewalks a minimum of 5 feet wide shall be provided along all public street frontages.

Clearly defined and lighted pedestrian walkways shall extend between parking areas and all building entrances.



Sidewalk Landscaping

Street trees shall be planted within a landscaped parkway or in tree pits within the sidewalk area according to City standards. Street trees shall be spaced between 30 and 40 feet apart.

Street trees planted within the sidewalk area shall be planted using the latest, most advanced horticultural techniques such as "CU Structural Soil," drip irrigation, etc.

Where buildings front the sidewalk (such as commercial, office and mixed use facilities) planter pots, planter boxes or hanging planter baskets shall be included and integrated into the buildings' design to provide seasonal color and enhance the pedestrian experience.

Where present, all front yard areas shall be landscaped with trees, shrubs, ground cover, and turf grass.

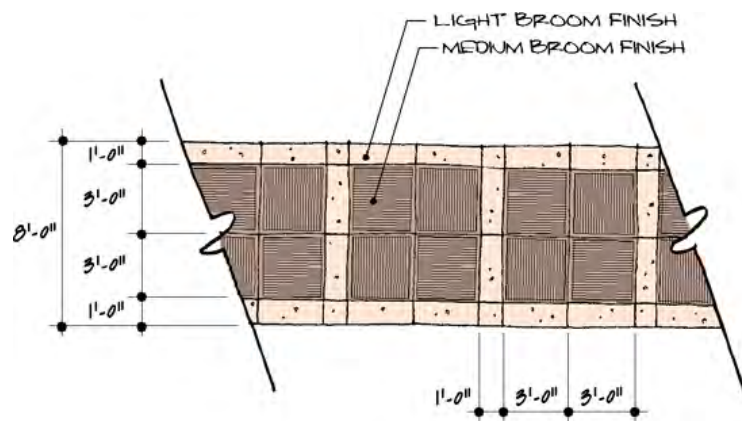


Sidewalks on Parkway Streets

Public sidewalks on parkway streets shall be a minimum of 8 feet wide, and constructed of integrally colored concrete, scored in an alternating band and panel arrangement and textured with both a smooth and medium broom finish. Joints shall be troweled, not saw cut.

As parkway streets, Bell Street and Fifth Street have an expanded right-of-way width and serve as collector streets. This provides for wider parkways to accommodate wider sidewalks, as well as landscaping and streetscape furnishings.

(see cross sections of streets in Master Plan)

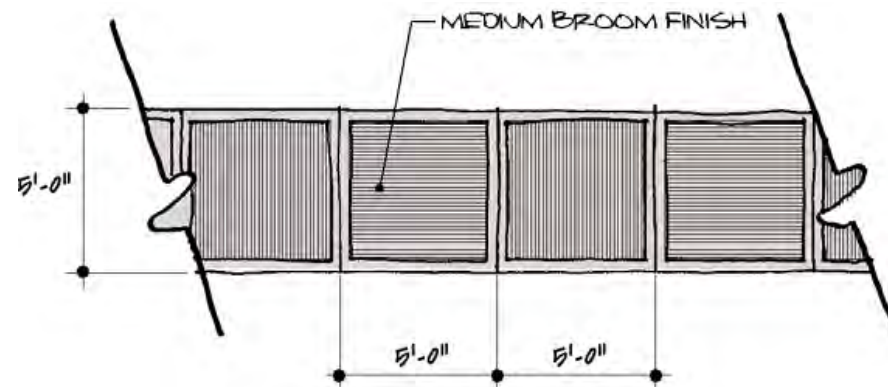


Sidewalks on Local Streets

Public sidewalks on local streets shall be a minimum of 5 feet wide and constructed of concrete. The surface shall consist of a stiff broom finish center field surrounded by smooth steel-troweled edge bands. Joints shall be troweled, not saw cut.

Walkways

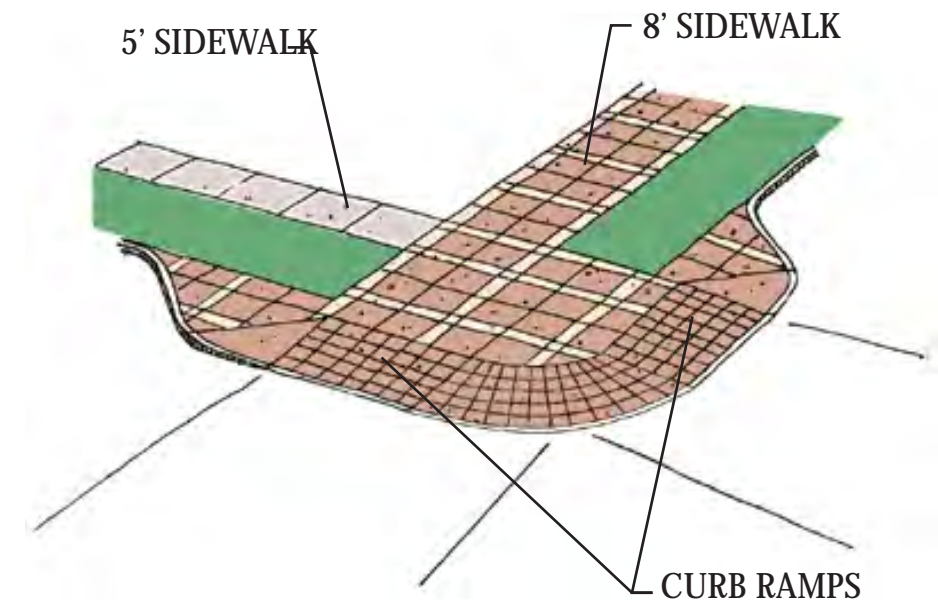
Walkways in parking areas, around buildings or on private properties shall be a minimum of 4 feet wide and constructed of either colored concrete or brick pavers. Bituminous walkways are prohibited.



Accessibility and Curb Ramps

All sidewalks and walkways shall meet the Americans with Disabilities Act (ADA) standards for accessibility.

Where curb ramps are provided, they shall comply with City standards and shall direct pedestrian flow in a continuous manner. They shall be constructed of integrally colored concrete, scored in a one-foot-square grid pattern and textured in a stiff broom finish.



Expanded full corner curb ramp for use at high pedestrian traffic areas.

Streetscape Furnishings

Public rights-of-way within the Port of Dubuque will be designed to provide visitors (pedestrians, bicyclists and motorists) with a safe, convenient, comfortable and attractive environment to circulate through and inhabit. The public streetscape will serve as an outdoor room where social discourse and exploration combine with vehicular access and mobility. To that end, a family of attractive, durable, benches, litter receptacles, bike racks, directory kiosks, and light fixtures are proposed to be located along the streets, parking areas and parks and plazas. See Appendix for product descriptions.



Outdoor Cafe Table and Chair



Painted Metal Woven Seat Bench



Litter Receptacle 1



Artist Carved Granite Bench



Artist Cast Utility Cover



Parking Lot Lighting



Painted Metal Bench



Wood Bench



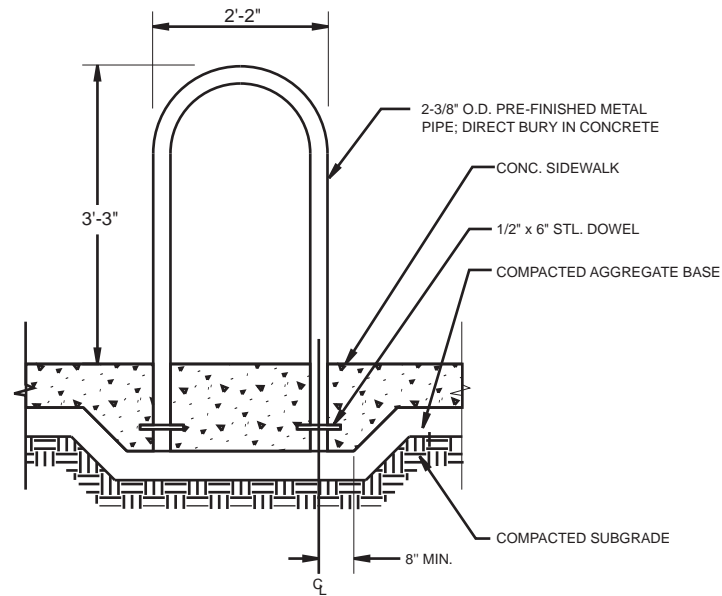
Litter Receptacle 2

Public Art

The use of public art to enliven and enrich the public and semi-public realm is encouraged. Consideration should be given during the site planning and project review phases to incorporating works of public art into the overall design of a particular development project. Proposed public art works shall be subject to the same review and approval procedures as for other projects described herein in these standards. See Applicability.

Bike Parking

Bike loops for parking at least 3 bicycles shall be provided within 20 feet of the main entry of commercial, mixed-use and public buildings.



Painted bike loops

Sidewalk Lighting

Public sidewalks and private walkways shall be illuminated with pedestrian scale light fixtures (12 feet tall). Illumination shall be provided by metal halide lamps arranged to produce a fairly even distribution of light at levels of approximately one footcandle.

See Appendix for light fixture information.



Single globe street and walkway light.

Parking Lot Lighting

All surface parking lots shall provide a generally even illumination pattern with an average illumination of three to five footcandles.

Illumination shall be accomplished with a combination of commercial grade parking lot and pedestrian style fixtures. Pedestrian fixtures shall be used for lighting internal parking lot walkways while parking lot fixtures shall be employed to illuminate parking bays and drive aisles.

The commercial grade parking lot fixtures shall be comprised of circular “hat-box” or “hockey puck” style luminaires in either single or double sets, attached to 25 foot tall, tapered metal poles by horizontal arms. Fixtures shall utilize metal halide lamps. New light poles located within parking bays shall be mounted on 2 foot high, limestone patterned, integrally colored concrete bases.



Double luminaire “hatbox” parking lot light on limestone patterned concrete base.

Parking Lot Landscaping

A landscaped buffer strip at least 7 feet wide shall be provided between all surface parking areas and the sidewalk or street. The buffer strip shall consist of shade trees, low shrubs and/or perennial flowers (3 feet in height max.), and a decorative metal fence no more than 4 feet high supported between decorative masonry columns. Landscaped earth berms are not permitted and shall not substitute for the landscape screening described above. Use of bio-filtration methods of landscape and drainage design are encouraged.*

A landscape buffer at least 7 feet wide shall be provided along the rear and sides of all surface parking lots. This area shall be planted with shade trees, coniferous trees, and a continuous 4 foot tall shrub border or hedge of deciduous and/or evergreen shrubs.

Parking bays in excess of 11 spaces in length shall be divided by intermediate landscaped islands at intervals of 11 spaces.

For single parking bays, landscaped islands shall provide at least one parking space of landscape area (9 x 18 feet) and shall be planted with one ornamental/dwarf tree, low shrubs, perennial flowers and/or ground cover/ornamental grasses (3 feet in height max.).

For double parking bays, both the end landscaped islands and the intermediate landscaped islands shall provide a double parking space of landscape area (9 x 36 feet) and shall be planted with one shade tree or two ornamental/dwarf trees, low shrubs, perennial flowers and/or ground cover/ornamental grasses (3 feet in height max.).

All new parking lot landscaping shall comply with City standards, be mulched to a depth of 4 inches with a high quality, finely shredded hardwood mulch and shall be watered by an automatic, underground irrigation system.

See Appendix.



Decorative fence between public sidewalk and parking.

Surface Parking

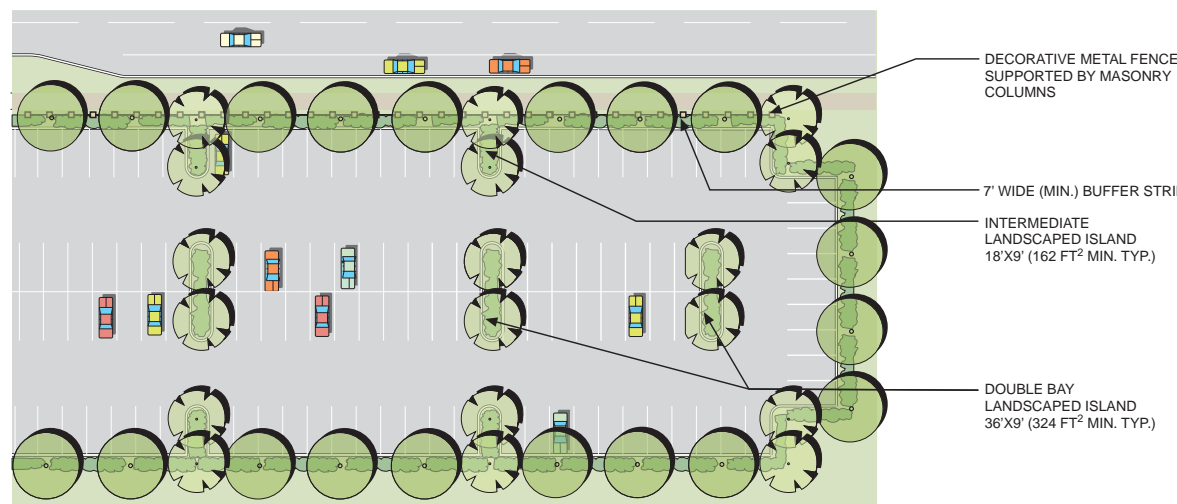
Off-street parking lots shall be designed and constructed utilizing civil engineering “best practices.” At a minimum, surface lots shall be paved with asphaltic concrete over a compacted aggregate base with edges and planting islands defined by concrete curb and gutter and parking spaces defined with painted striping. Parking on gravel, dirt or unreinforced turf is prohibited.

Off-street parking shall be located to the rear and/or side of buildings. Front-yard parking is permissible only along a maximum of 20 percent of the total site frontage.

When parking or parking access must be located in the front yard, a landscaped buffer shall be provided. See Parking Lot Landscaping.



Examples of well landscaped parking areas.



Example of adequate parking lot landscape layout.

Off-Street Parking Requirements

The intent of this section is to provide flexibility in the determination of the number of off-street parking spaces required to meet the minimum needs of each proposed building or use while taking into account the efficiencies inherent in the differing peak demand requirements of these uses and the overall sharing of parking facilities between uses. Furthermore, it shall be understood that there are existing shared parking agreements in place which may require amending in order to accommodate and facilitate the addition of new buildings and uses.

The number of off-street parking spaces required for each building or use shall be determined through the design review process for each individual project.

Refuse

Outdoor placement of freestanding dumpsters or refuse containers is prohibited. Refuse shall be securely stored and enclosed as part of the principal structure, and contained and disposed of in accordance with applicable state, county and city health, safety and environmental codes. Use of sealed compactors is encouraged.

Where food service/restaurant kitchen refuse storage is not accommodated as part of the principal building, refuse storage is permissible in an accessory storage enclosure. See Accessory Structures/Buildings.

Fences and Screen Walls

All fencing and screen walls shall be designed to incorporate intermediate structural components in order to avoid a “front” and “back” side. Fencing and screen walls shall be attractive from all views.

Residential Uses

Fencing placed in front yard areas between the sidewalk and the building shall be up to 42 inches in height. The following materials are acceptable in front yard locations:

- Painted architectural metals, including heavy gauge metal mesh in a square or rectangular configuration, with a painted metal frame.
- Fencing may be mounted on decorative masonry wall of brick, stone, or architectural precast concrete units. Total height of wall and fence shall not exceed 42 inches. Masonry walls shall be of durable, permanent construction. Dry-laid or stacked construction is not permissible.

Unacceptable materials for front yard residential uses include:

- Wood fencing of any kind.
- Chain link fencing.
- Fiberglass or plastic pickets or panels.

Rear or side yard areas may have privacy fencing up to 6 feet in height.

Acceptable materials for rear and side yard residential uses include:

- Solid wood board panels of rot-proof lumber such as cedar, teak, or treated pine. Decorative wood lattice may occupy the top portion of the fence up to a maximum of 18 inches.
- Painted wood or plastic picket fencing.
- Any of the acceptable front yard fence and/or wall materials.

Unacceptable materials for rear and side yard residential uses include:

- Chain link fencing.
- Fiberglass or plastic panels.
- Plywood or chip board panels.
- Corrugated or sheet metal panels.

Office and Commercial Uses

Decorative fencing for office and commercial uses in areas facing the primary street, such as entry courtyards or outdoor seating areas, may be located between the building and the sidewalk.

Fencing in these areas shall be limited to the following:

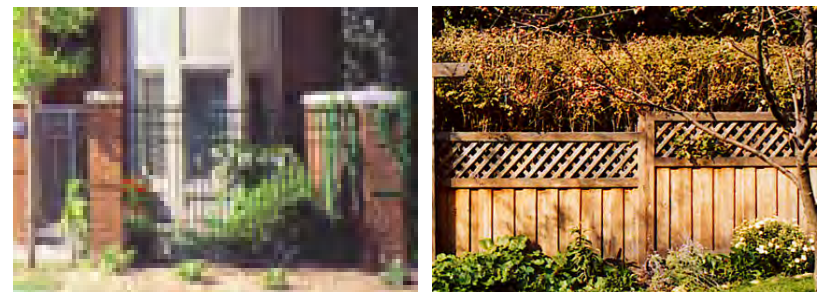
- Painted architectural metals, including heavy gauge metal mesh in a square or rectangular configuration, with a painted steel frame.
- Fencing may be mounted on decorative masonry wall of brick, stone, or architectural precast concrete units. Total height of wall and fence shall not exceed 42 inches. Masonry walls shall be of durable, permanent construction. Dry-laid or stacked construction is not permissible.

Unacceptable materials for front areas of office and commercial uses include:

- Wood fencing of any kind.
- Chain link fencing.
- Fiberglass or plastic pickets or panels.
- Corrugated or sheet metal panels.



Commercial or office fencing examples.



Residential fencing examples.

Pre-Development Temporary Fencing

Existing uses that require screening for outdoor storage and mechanical equipment shall be a minimum of 8 feet in height, with a maximum height of 10 feet. Acceptable materials include:

- Solid wood panels - painted or stained and sealed.
- Fabric (See Appendix for product listing).

Unacceptable materials for temporary screening include:

- Chain link fence.
- Fiberglass or plastic panels.
- Corrugated or sheet metal panels.

See Parking Lot Landscaping for fencing in parking lot areas.

Outdoor Storage

Outdoor storage of any kind including vehicles, raw materials, etc. is prohibited. Seasonal equipment, supplies, etc. shall be either stored within each facility or within a permanent accessory structure. See Accessory Structures/Buildings.

Outdoor Audio

Music, public address announcements and other forms of outdoor audio are discouraged. Sound shall be contained within the site boundaries. Outdoor music or other forms of outdoor audio associated with special events shall be controlled by the City's regulations for special events.

Newspaper Boxes

Newspaper dispensing units shall be clustered in small groups (2 or 3) and be ground-mounted, not chained or strapped to other streetscape elements.

Vending Machines

Outdoor placement of vending machines including but not limited to beverages and snacks is prohibited. Vending machines shall be located indoors.

Signs

Building and vessel signs shall be architecturally compatible with the style, composition, materials, colors and details of the building or vessel. Signs shall be an integral part of the building and site design.

A comprehensive sign program shall be developed for buildings or vessels which house more than one business. Signs shall be compatible with one another. No more than two types of signs shall be used on a single building facade or vessel (i.e. wall signs, projecting signs, awning signs).

Sign Location: Wall signs on commercial or mixed-use storefront-type buildings shall be placed within a “sign band” immediately above the storefront display windows or entryway. Wall signs on other building types shall be placed where they do not obscure architectural features.

Sign Materials: Sign materials shall be consistent or compatible with the construction materials and architectural style of the building facade on which they are to be displayed. Neon signs are permissible for display windows.

Sign Illumination: Both internal and external illumination of signs, except for awnings, is permissible.

The following sign types are prohibited:

- Pole or pylon signs
- Billboards
- Internally illuminated awnings
- Flashing and rotating signs
- Portable signs
- Search lights
- Audible or musical signs
- Roof signs

The following sign types are permissible:

- Non-illuminated awning signs
- Canopy or marquee signs
- Wall signs
- Projecting signs
- Window signs
- Freestanding ground/monument signs
- Freestanding non-flashing, electronic information or message signs*

*Limited to public and institutional uses such as a conference center or museum if incorporated into an overall facility identity sign.

Projecting Signs:

Maximum dimensions for projecting signs: 9 square feet, with a clearance of at least 8 feet from ground level, projecting no more than 3 feet from the side of the building. Maximum: 1 per business.

Window Signs:

Window signs shall cover no more than 10 percent of the window area.

Directional Signs:

Directional signs shall be limited to 6 SF per sign and allowed as needed subject to the overall review and approval of a comprehensive sign program.



Well-coordinated wall and awning signs.



Freestanding Ground/Monument Signs:

Freestanding ground/monument signs shall be limited to use by commercial and institutional establishments such as hotels, restaurants, gas stations, museums, etc. Sign design materials, colors and detailing shall be similar to those of the principal building.

Maximum dimensions for ground/monument signs:

- Maximum height: 20 feet
- Maximum structure area any one side: 135 SF
- Maximum structure total area all sides: 460 SF
- Maximum sign area any one side: 75 SF
- Maximum total (aggregate) sign area per premises: 250 SF
- Maximum number: 1 sign per premises

Electronic Message Signs:

Maximum number: 1 sign per facility.

Maximum sign size: 75 SF



Wall Signs:

Commercial/retail wall signs:

- Maximum sign area per use or occupant: 50 SF
- Maximum number: 1 sign per business
- Maximum aggregate sign area per premises or building: 100 SF
- Wall signs shall be limited to no more than 3 colors

Restaurant, nightclub or entertainment facility wall signs:

- Maximum sign area per use: 100 SF
- Maximum number: No limit, up to aggregate area per premises
- Maximum aggregate sign area per premises or building: 400 SF

Hotel, gaming and institutional or public building wall signs:

- Maximum sign area per use: 250 SF
- Maximum number: No limit, up to aggregate area per premises
- Maximum aggregate sign area per premises or building: 500 SF

Cinema and theater signs:

- Marquee sign: maximum 3 sides, maximum 300 SF of changeable information area plus 150 SF identity signs for a maximum aggregate area of 450 SF
- Maximum size of wall sign: 32 SF
- Maximum aggregate sign area for all wall signs per premises (not including marquee): 64 SF



Vessel Signs:

Maximum sign area: 10% of vessel area or 500 aggregate SF, whichever is less.
Maximum number: No limit, up to aggregate area per vessel.
Vessel area shall be calculated as length times height of vessel.
Height of vessel shall be defined as the mean waterline to the top of the upper deck not including the pilot house.

Note: Required US Coast Guard vessel names are excluded from these requirements.

GLOSSARY

Accessory Building:

A subordinate building, located on the same lot as the main building, or a portion of the main building, the use of which is clearly incidental to and customarily found in connection with the main building or principal use of the land.

Arbor:

A rooftop or garden structure which provides shade.

Arcade:

A roofed passageway, usually with shops on one or both sides.

Building Frontage:

The front facade of a building, typically abutting the sidewalk.

Cornice:

Any projecting ornamental moulding along the top of a building or wall.

Eaves:

The underpart of a sloping roof overhanging a wall.

EFIS (Exterior Finish Insulation System):

A coating of cementitious, stucco-like material over rigid insulation.

Entertainment Facility:

A business engaged in the provision of legal activities or performances for the enjoyment of the public.

Franchise Architecture:

Building design that is trademarked or identified with a particular chain or corporation and is generic in nature.

Infill Building:

A new building sited within an established neighborhood, often between two existing buildings.

New Building:

In addition to actual new buildings, the term “new building” shall include any expansion of an existing building that equals or exceeds 50 percent of the original building’s floor area and shall also include buildings which are the subject of substantial material and comprehensive renovation.

Parapet:

A low, decorative wall or railing along the edge of a roof.

Gable Roof:

A pitched roof with a central ridge line and vertical wall ends.

Gambrel Roof:

A roof with a double pitch terminating in a small gable at the ridge.

Hip Roof:

A pitched roof with sloped instead of vertical ends.

Mansard Roof:

A pitched roof having a double slope, the lower pitch being longer and steeper than the upper.

Office/Showroom:

A product display or showroom facility with two primary uses: office and showroom/display space. Storage, light assembly or shipping and receiving may occupy no more than 20% and office use may occupy no more than 75% of the facility’s gross floor area.

Primary Facade:

The facade fronting a public street. In the case of corner buildings, the primary facade fronts the highest classification of street.

Renovation:

1) Any exterior remodeling and/or site alteration of 25% or more of any existing vessel, commercial, office, multifamily or institutional building or structure; or 2) any exterior remodeling that clearly alters the appearance of such a building or structure, including change in exterior paint color or material; or 3) any interior remodeling of 50% or more of existing building floor area.

Service Commercial:

Commercial uses that are primarily oriented to service rather than retail sales, such as barbers, photocopying, photo development, dry cleaners, shoe repair, etc.

Sidewalk Line:

The outer edge of the sidewalk, where the front facade of a storefront building is typically located.

Storefront Building:

A commercial building located at the sidewalk line, with display windows and principal entry on the ground floor facing the sidewalk. Storefront buildings typically include traditional elements such as a sign band above the storefront, a transom, a recessed entry and a kickplate as a base to the storefront.

Streetscape:

The public right-of-way, from building face to building face, occupied by the street, parkway, sidewalk and pedestrian amenities such as lighting, benches, bike racks, etc.

Port of Dubuque

APPENDIX

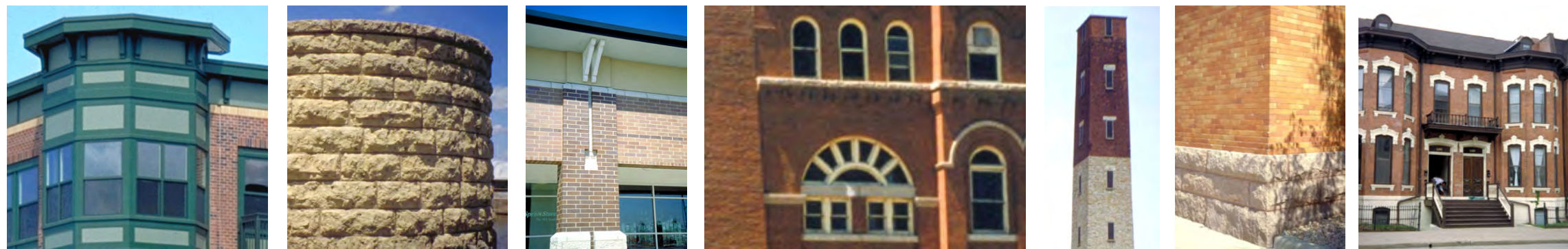
Adopted March 4, 2002



Urban
Context



Building
Massing &
Proportion



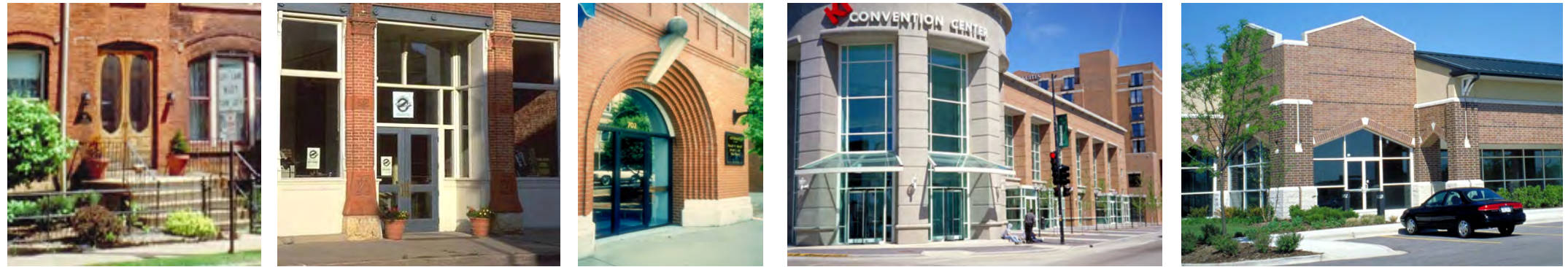
Materials
& Color



Building
Ornament
& Detail

Design Vocabulary

Entries



Windows



Building
Signs &
Awnings



Site
Signs



Design Vocabulary



Street &
Sidewalk



Streetscape
Elements



Streetscape
Elements



Transit
Facilities

Design Vocabulary

Parking



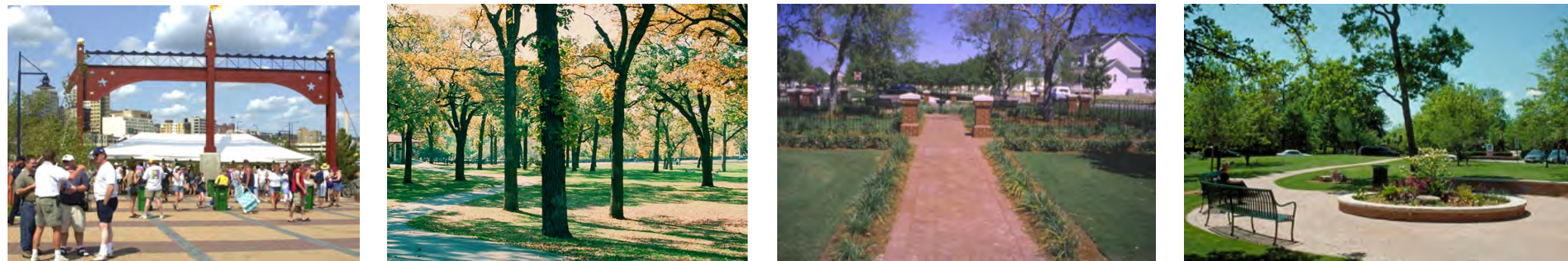
Parking



Open Space

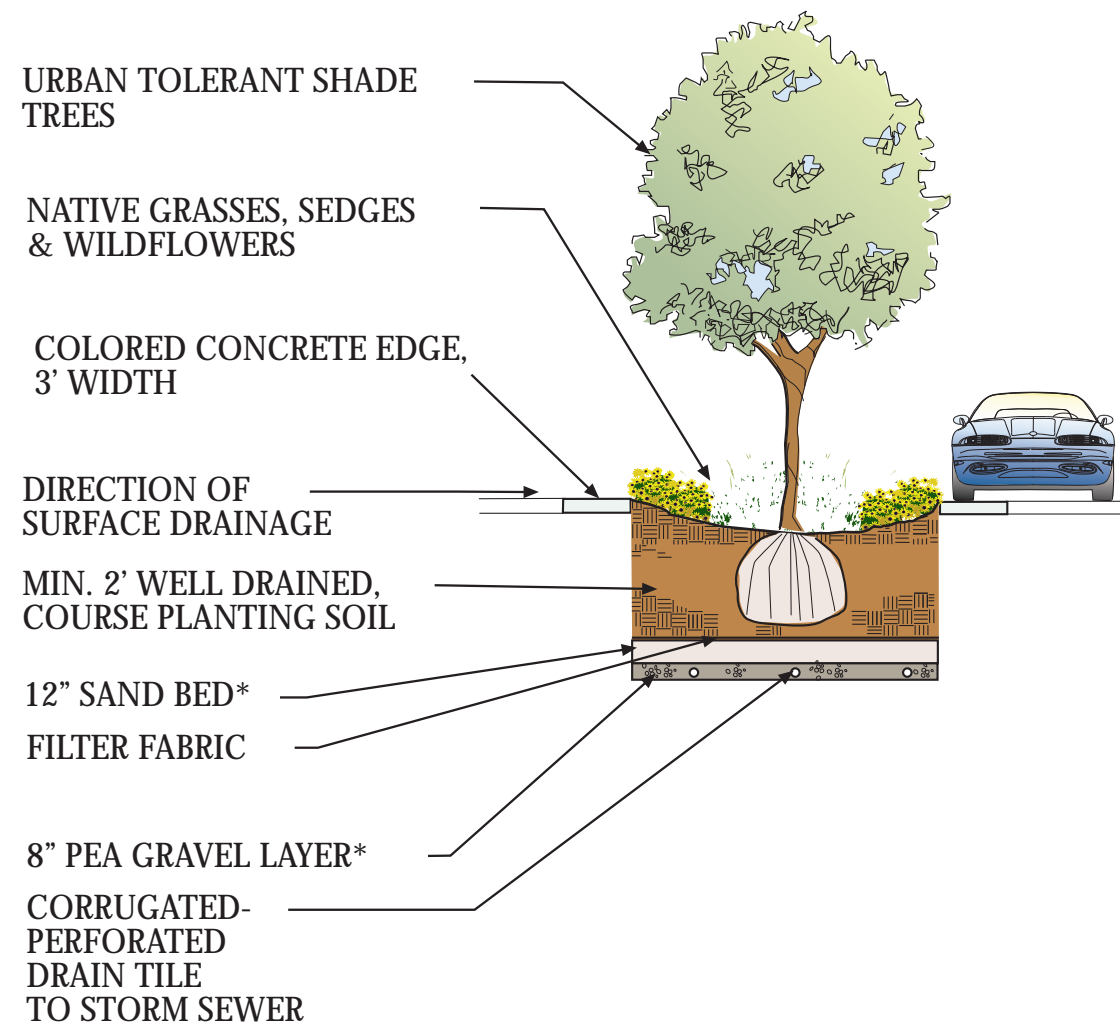


Open Space



Design Vocabulary

Parking Lot Landscape Concept



** All dimensions are approximate, final design shall be based on design storm event and existing subsurface soil characteristics*

CONCEPTUAL DESIGN OF VEGETATIVE SWALE FOR PARKING AREAS



BUTTERFLY WEED



INDIAN GRASS



LIATRUS



BLACKEYED SUSAN



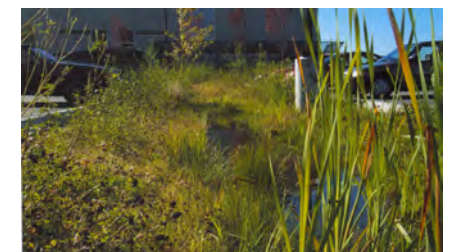
MARSHALL SEEDLESS ASH



SWALE CONSTRUCTION



VEGETATIVE SWALE



VEGETATIVE SWALE

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Claytor, Richard A. and Thomas R. Schueler. 1996. Design of Stormwater Filtering Systems. Chesapeake Research Consortium. Available from The Center for Watershed Protection at 410-461-8323

Minnesota Pollution Control Agency. 2000. Protecting Water Quality in Urban Areas; Best management practices for dealing with storm water runoff from urban, suburban and developing areas in Minnesota. Minnesota Pollution Control Agency. Available from the MPCA contact Mary Osborn at 651-296-7523

Pre-Development Temporary Fencing

The following products are pre-approved by the City for use at the Port of Dubuque; however, it shall be understood that the items listed are all subject to an “or equal” review process. Product substitutions of equal design, quality, and performance are permissible per the review and approval of the City.

Mesh Banner: Staftex #5071-291 (PVC mesh).
Width: 124” and 197.”
PVC coated net fabric.
Weight: approximately 9.14 oz/sq. yard.
Tensile strength: 472/360 lb. tear-resistance warp/weft: 67/90 lb.
Elongation at break: 14%/20%.
Adhesion: HF-weldable.
Cold resistance: -30° c.
Air permeability: at least 1,500 1/m/sec.
Flame retardant: B1 Flame Retardant.
Application: net.
Visual Impact Signs, Inc. 8732 West 35W Service Drive NE, Blaine, MN. Phone: 877-783-9411.

Recommended Plant List

Trees

Autumn Blaze Maple	Acer x freemanii ‘Jeffers Red’
River Birch	Betula nigra
Hackberry	Celtis occidentalis
Marshall Seedless Ash	Fraxinus pennsylvanica ‘Marshall’s Seedless’

Shrubs

Black Chokeberry*	Aronia melanocarpa
Isanti Dogwood*	Cornus sericea ‘Isanti’
Smooth Wild Rose	Rosa blanda
Meadowsweet	Spiraea alba
Western Snowberry	Symphoricarpos occidentalis

Grasses

Bebb’s Sedge	Carex bebbii
Fox Sedge	Carex vulpinoidea
Soft Rush	Juncus effusus
Torrey Rush	Juncus torreyi
Little Bluestem	Schizachyrium scoparium
Prairie Dropseed	Sporobolus heterolepis

Flowers

Yarrow	Achillea ‘Moonshine’
Marsh Milkweed	Asclepias incarnata
Alert Red Aster	Aster novi-belgii ‘Alert’
New England Aster *	Aster novae-angliae
Joe Pye Weed *	Eupatorium maculatum
Meadow Blazing Star	Liatris ligulistylis
Great Blue Lobelia	Lobelia siphilitica
Bee Balm	Monarda didyma ‘Marshalls Delight’
Obedient Plant	Physostegia virginiana
Black Eyed Susan	Rudbeckia fulgida ‘Goldsturm’
Spiderwort	Tradescantia ohimensis
Culver’s Root*	Veronicastrum virginicum
Golden Alexanders	Zizia aurea

* Plants that should be reserved for buffer and screening areas because they grow over 3 feet in height and may inhibit surveillance.

Streetscape Furnishings List

The following products are preapproved by the City for use at the Port of Dubuque; however, it shall be understood that the items listed are all subject to an “or equal” review process. Product substitutions of equal design, quality and performance are permissible per the review and approval of the City.

Cafe Table: “Steelhead” 36” table top with “Catena” surface mount base, Grotto powdercoat, Landscape Forms, Kalamazoo, MI. Phone: 800-521-2546

Cafe Chair: “Verona” without armrests, Grotto powdercoat, Landscape Forms, Kalamazoo, MI. Phone: 800-521-2546

Painted Metal Woven Seat Bench: “Scarborough Backed Bench” 72” long, Grotto powdercoat, Landscape Forms, Kalamazoo, MI. Phone: 800-521-2546

Litter Receptacle 1: “Scarborough” receptacle, side opening, Grotto powdercoat, Landscape Forms, Kalamazoo, MI. Phone: 800-521-2546

Artist carved granite bench example: Stanton Sears for the City of Minneapolis, Nicollete Mall Renovation

Artist cast iron utility cover example: Kate Burke for the City of Minneapolis, Nicollete Mall Renovation

Parking Lot Light Fixture: “Form Ten” round, arm mounted luminaire by Gardco, CA 22” Style with minimum 250 watt Metal Halide lamp, single or double head on a tapered steel 25’ pole, powdercoat RAL 9017 (black) smooth gloss finish.

Painted Metal Bench: “Hyde Park” model# HP3005-BS-75, metal rod seat, Grotto powdercoat, Landscape Forms, Kalamazoo, MI. Phone: 800-521-2546

Wood Bench: “Giverney” bench 6’ model #U5547 by Smith and Hawken, Novato, CA. Phone: 800-423-0117

Litter Receptacle 2: “Presidio, top-opening” Grotto powdercoat, Landscape Forms, Kalamazoo, MI. Phone: 800-521-2546

Street and Walkway Light: “Lumec” single acorn globe minimum 100 watt Metal Halide lamp on traditional 12 foot tapered steel pole, Powdercoat RAL 9017 (black) smooth gloss finish, model # L52 LMS 13004A